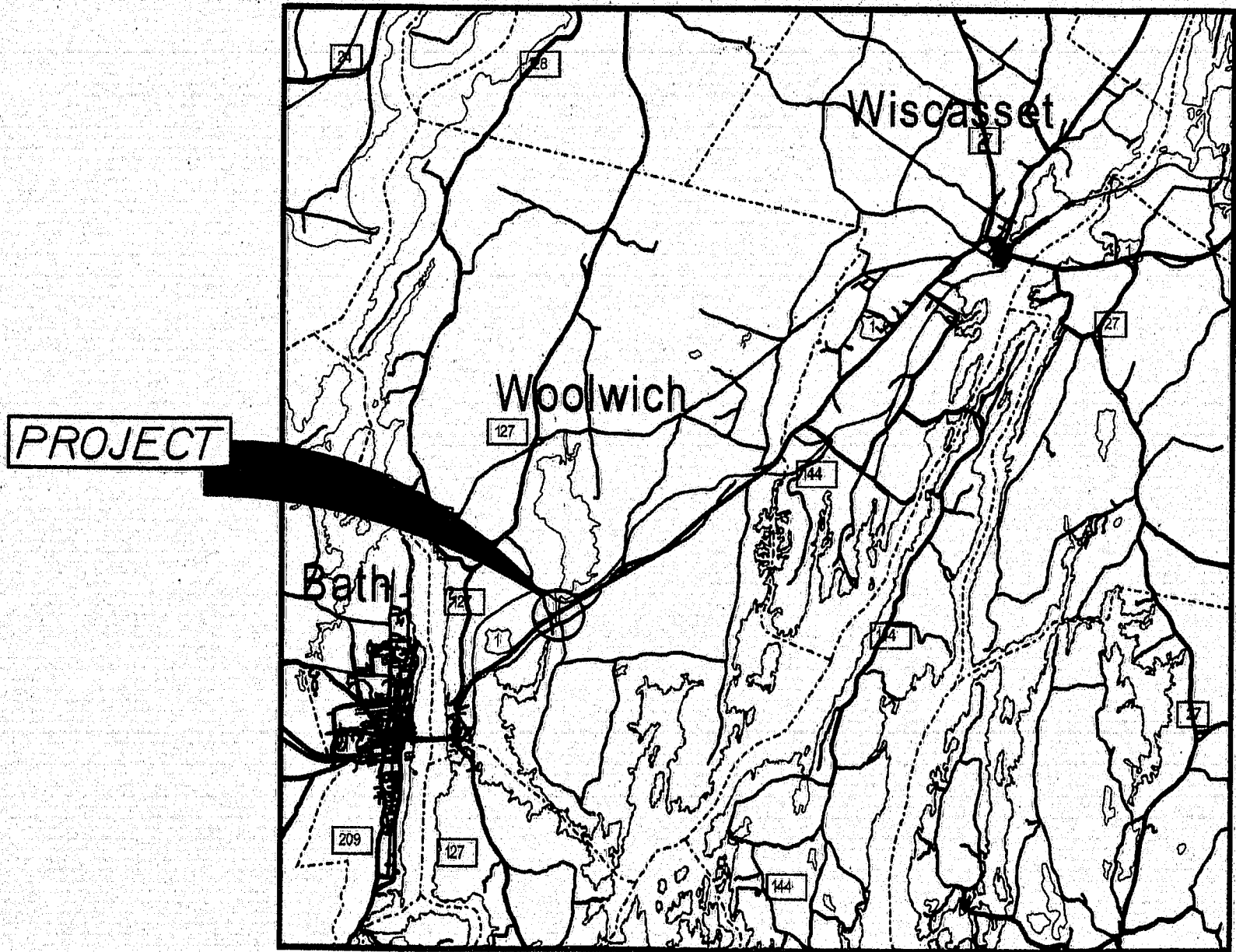


STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION

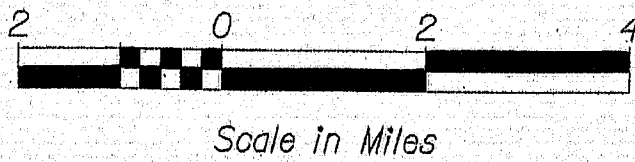


WOOLWICH  
SAGADAHOC COUNTY  
NEQUASSET BRIDGE  
OVER  
NEQUASSET OUTLET  
ROUTE 1

PROJECT NO. BH-1262(800)X  
PROJECT LENGTH 0.07 mi.  
BRIDGE DECK REPLACEMENT  
BRIDGE NO. 5695



LOCATION MAP



SPECIFICATIONS

Design: AASHTO LRFD Bridge Design Specifications, Fourth Edition 2007.

DESIGN LOADING

Live Load ..... HL - 93 Modified

TRAFFIC DATA

Current (2005) AADT ..... 20,550  
Future (2025) AADT ..... 28,770  
DHV - % of AADT ..... 10  
Design Hour Volume ..... 2,877  
% Heavy Trucks (AADT) ..... 5  
% Heavy Trucks (DHV) ..... 4  
Directional Distribution (DHV) ..... 58  
18 kip Equivalent P 2.0 ..... 583  
18 kip Equivalent P 2.5 ..... 556  
Design Speed (mph) ..... 45

MATERIALS

Concrete (Unless noted otherwise) ..... Class "A"  
Concrete (Precast) ..... Class "P"  
Concrete (Curbs, & Transition Barriers) ..... Class "LP"  
Reinforcing Steel ..... ASTM A615/A615M, Grade 60  
Structural Steel:  
All Material (except as noted) ..... ASTM A709/A709M, Grade 50W  
High Strength Bolts ..... ASTM A325, Type 3  
Prestressing Strand ..... AASHTO 203 (ASTM A416),  
Grade 270, Low Relaxation

BASIC DESIGN STRESSES

Concrete .....  $f'c = 4,350$  psi  
Reinforcing Steel .....  $f_y = 60,000$  psi  
Structural Steel:  
ASTM A 709/A 709M, Grade 50W .....  $F_y = 50,000$  psi  
ASTM A 709/A 709M, Grade 36 .....  $F_y = 36,000$  psi  
ASTM A 325 .....  $F_u = 120,000$  psi  
Prestressing Strand .....  $F_u = 270,000$  psi

UTILITIES

NONE

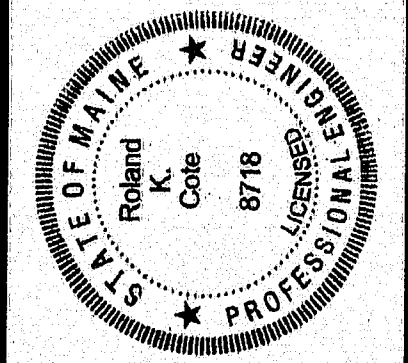
MAINTENANCE OF TRAFFIC

Maintain Two way traffic detoured onto George Wright Road.

LIST OF DRAWINGS

|  |    |
|--|----|
| Title Sheet .....                        | 1  |
| Quantities & Notes .....                 | 2  |
| General Plans .....                      | 3  |
| Profile .....                            | 4  |
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| Abutment No. 1 Details .....             | 6  |
| Abutment No. 1 Reinforcing Steel .....   | 7  |
| Abutment No. 2 Plan .....                | 8  |
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| Superstructure Plan .....                | 10 |
| Superstructure Details .....             | 11 |
| Superstructure Reinforcing Steel .....   | 12 |
| Superstructure Reinforcing Details ..... | 13 |
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|  |                    |          |
|--|--------------------|----------|
| STATE OF MAINE<br>DEPARTMENT OF TRANSPORTATION | APPROVED           | DATE     |
| COMMISSIONER                                   | <i>[Signature]</i> | 11/16/07 |
| CHIEF ENGINEER                                 | <i>[Signature]</i> | 11/08/07 |



|                    |             |          |
|--------------------|-------------|----------|
| SIGNATURE          | P.E. NUMBER | DATE     |
| <i>[Signature]</i> | 8718        | 11/17/07 |

|         |                 |                  |            |                  |            |                         |
|---------|-----------------|------------------|------------|------------------|------------|-------------------------|
| PROGRAM | PROJECT MANAGER | DESIGNER         | CONSULTANT | PROJECT RESIDENT | CONTRACTOR | PROJECT COMPLETION DATE |
| BRIDGE  | BEN FOSTER      | ROBERT K. FOSTER |            |                  |            |                         |

|   |
|---|
| WOOLWICH<br>NEQUASSET BRIDGE<br>TITLE SHEET |
|---|

|              |
|--------------|
| SHEET NUMBER |
| 1            |
| OF 15        |

BH-1262(800)X PIN 12628.00

145-326



[illegible][illegible]

GENERAL CONSTRUCTION NOTES  
FOR DECK REPLACEMENT

1. During construction, the road will be closed to traffic for a time period specified in the Special Provisions.

2. Benchmark - Use the top of beam Elevation of 0.00' at the intersection of the  $\odot$  Construction line and the  $\odot$  Bearing of Abutment No. 1 for all Elevations.

3. Do not excavate for Aggregate Subbase Course where existing material is suitable as determined by the Resident.

4. In areas where the Resident directs the Contractor not to excavate to the subgrade line shown on the plans, payment for removing existing pavement, grubbing, shaping, ditching, and compacting the existing subbase and layers of new subbase 6 inches or less thick will be made under appropriate equipment rental items.

5. Approach elevations shall be adjusted to provide a smooth transition onto and off of the Bridge.

6. Place loam 2 inches deep on all new or reconstructed sideslopes or as directed by the Resident.

7. The new Guardrail on Abutment No. 1 will be warped so to provide a safe transition from the existing guardrail and the Bridge Transition. The new Guardrail on Abutment No. 2 will be warped so to provide a safe transition from the Bridge Transition and anchored to the existing ledge.

8. Protective Coating for Concrete Surfaces shall be applied to the following areas:

*All exposed surfaces of concrete curbs.  
(Permanent Concrete Barrier Type III-B, if used)*

Fascias down to the drip notch,  
All exposed surfaces of Concrete Transition Barriers,  
Top of abutment backwalls and to 12 inches below the top of backwalls  
on the back side.

9. The Paint on the existing bridge is Lead Based. The top flanges of the stringers are coated with lead paint. This paint will need to be removed in the immediate area where shear studs are to be welded, and in all other areas where the Contractor is required to work on the existing steel. The Contractor is responsible for removing this paint in accordance with all applicable regulations and special provisions. There will be no separate payment for this work. The cost shall be considered incidental to the work requiring the paint removal.

10. Existing Deck shall be removed in its entirety - Property of the Contractor shall include all Deck, Railing, Guardrail and Barrier Concrete.

*11. Bidders and Contractors may obtain a copy of the existing bridge plans by contacting the Project Manager. The plans are reproductions of the original drawings as prepared for the construction of the bridge. It is very unlikely that the plans will show any construction field changes or any alterations which may have been made to the bridge during its life span.*

12. Minimum Lap Splice for #5 Bars shall be 22 inches, the Minimum Lap Splice for #6 Bars shall be 26 inches.

13. The removal of Superstructure items above the top flange of Beams and all Abutment Concrete above the elevation shown on the plans will be incidental to Item 202.17 Removal Existing Structural Concrete.

14. The Contractor has the option to choose to use Permanent Concrete Barrier, Type 111A in place of the Steel Bridge Rail, 2 Bar; 4-Concrete Transition Barriers; and Curbs. A new CB "6 Bar will replace bar S550 on the Reinforcing Steel Schedule. Standard Details shall apply. Payment will be made under Item No. 526.321. If Permanent Concrete Barrier is used no Payment shall be given to Steel Bridge Rail, 2 Bar; 4-Concrete Transition Barriers; and Curbs.

|                               |              |  |   |  |  |                    |  |  |
|-------------------------------|--------------|--|---|--|--|--------------------|--|--|
| <div>2</div> <div>OF 15</div> | SHEET NUMBER |  | NEQUASSET BRIDGE<br>NEQUASSET OUTLET<br>WOOLWICH SAGADAHOC COUNTY |  | PROJ. MANAGER<br>DESIGN-DETAILED<br>DESIGN-REVIEWED<br>DESIGN-DETAILED<br>REVISIONS 1<br>REVISIONS 2<br>REVISIONS 3<br>REVISIONS 4 | BEN. POSTER<br>RMS | DATE<br>DATE<br>DATE<br>DATE<br>DATE<br>DATE<br>DATE | STATE OF MAINE<br>DEPARTMENT OF TRANSPORTATION<br>BH-1262(800)X<br>PIN |
|                               |              |  | QUANTITIES AND NOTES  |  |  |                    |  |  |
|                               |              |  |   |  |  |                    |  |  |
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145.327

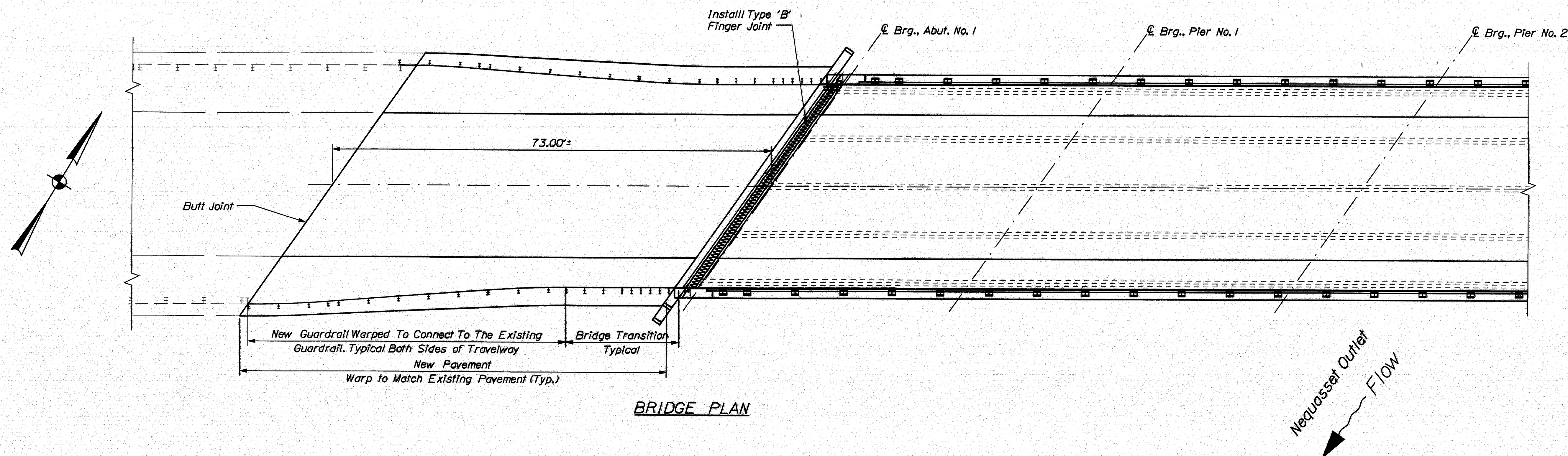


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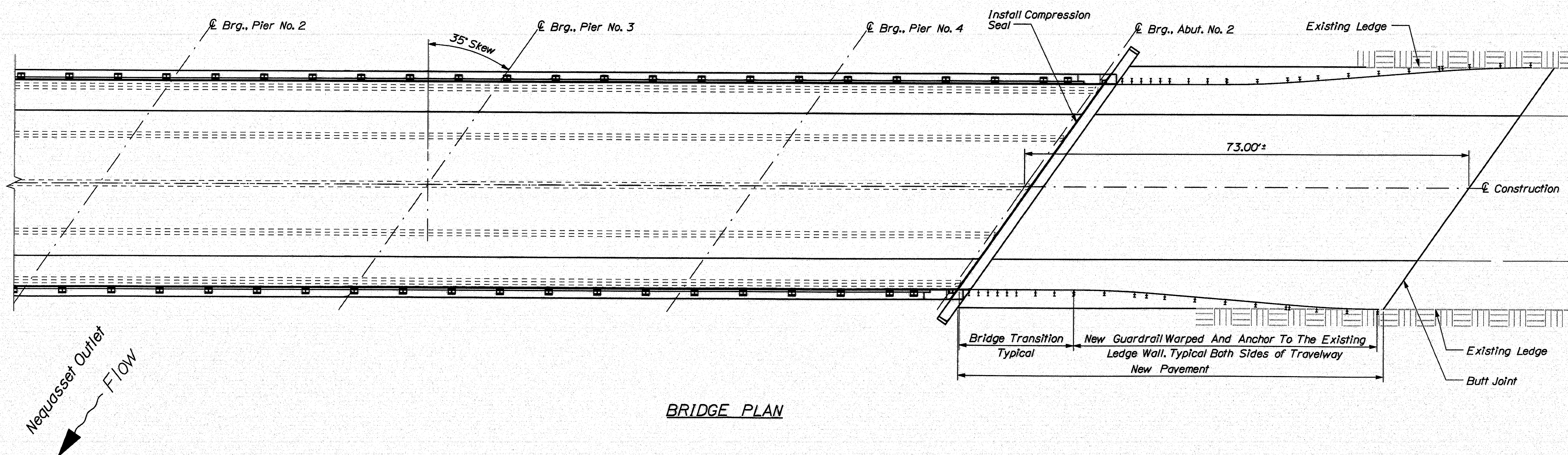
Division: BRIDGE

Username: david.sullivan

Date: 11/7/2007



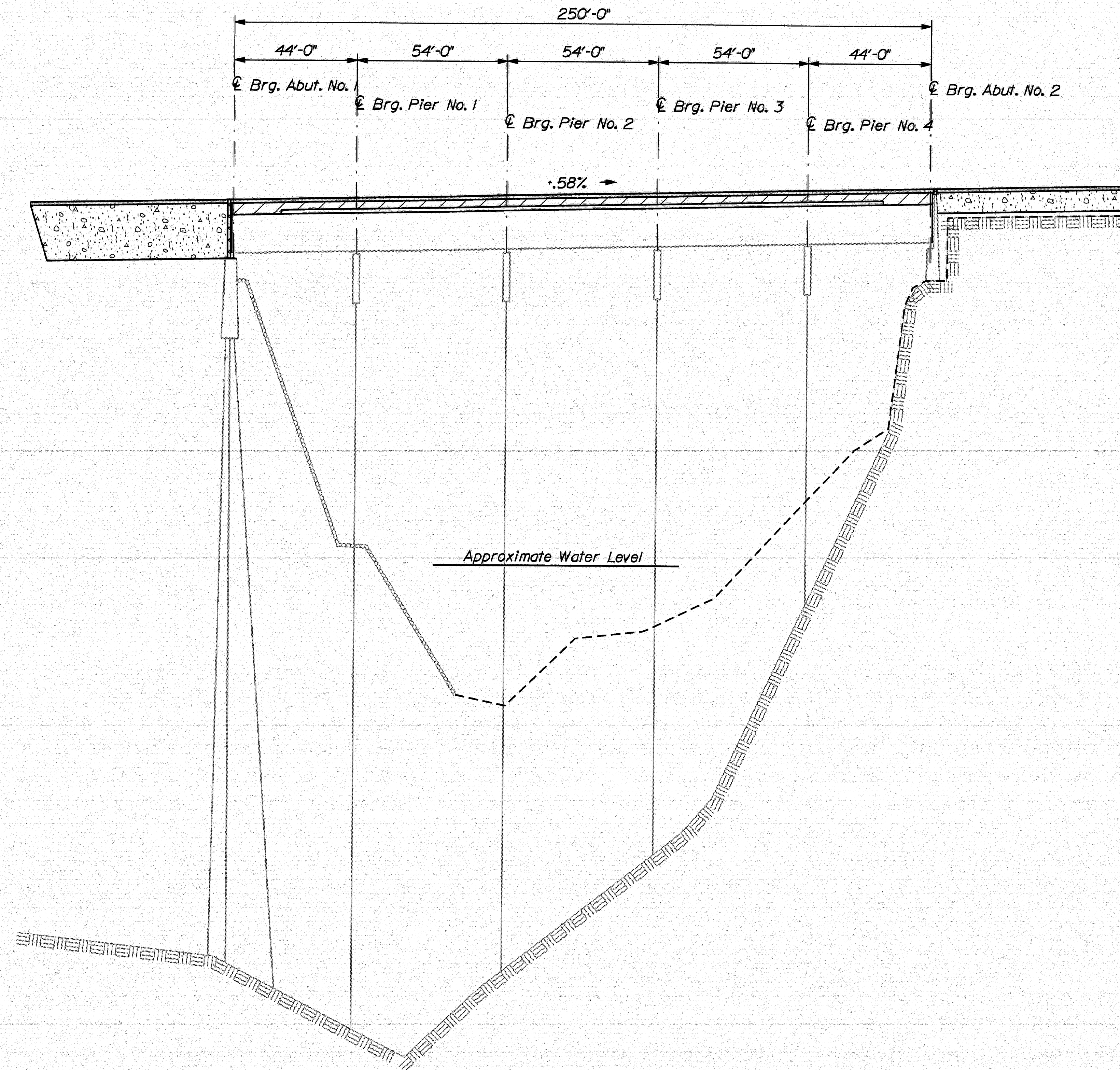
BRIDGE PLAN



BRIDGE PLAN

|               |       |                  |                  |                  |                              |               |     |     |             |      |  |
|---------------|-------|------------------|------------------|------------------|------------------------------|---------------|-----|-----|-------------|------|--|
| SHEET NUMBER  |       | NEQUASSET BRIDGE |                  | PROJ. MANAGER    |                              | BEN FOSTER    |     | BY  |             | DATE |  |
| 3             | OF 15 | WOOLWICH         | NEQUASSET OUTLET | SAGADAHOC COUNTY | DESIGN-DETAILED              | CHKD-REVIEWED | R/C | DNS |             |      |  |
|               |       |                  |                  |                  | DESIGN-DET ALEFD2            |               |     |     | SIGNATURE   |      |  |
|               |       |                  |                  |                  | DESIGN-DET ALEFD3            |               |     |     | P.E. NUMBER |      |  |
|               |       |                  |                  |                  | REVISIONS 1                  |               |     |     | DATE        |      |  |
|               |       |                  |                  |                  | REVISIONS 2                  |               |     |     |             |      |  |
|               |       |                  |                  |                  | REVISIONS 3                  |               |     |     |             |      |  |
|               |       |                  |                  |                  | REVISIONS 4                  |               |     |     |             |      |  |
| GENERAL PLANS |       |                  |                  |                  |                              |               |     |     |             |      |  |
|               |       |                  |                  |                  | STATE OF MAINE               |               |     |     |             |      |  |
|               |       |                  |                  |                  | DEPARTMENT OF TRANSPORTATION |               |     |     |             |      |  |
|               |       |                  |                  |                  | BH-1262(800)X                |               |     |     |             |      |  |
|               |       |                  |                  |                  | PIN                          |               |     |     |             |      |  |
|               |       |                  |                  |                  | BRIDGE NO. 5695              |               |     |     |             |      |  |
|               |       |                  |                  |                  | 12628.00                     |               |     |     |             |      |  |
|               |       |                  |                  |                  | BRIDGE PLANS                 |               |     |     |             |      |  |





PROFILE

145-329

|  |  |             |  |              |
|--|--|-------------|--|--------------|
| STATE OF MAINE<br>DEPARTMENT OF TRANSPORTATION           |  | SIGNATURE   |  | DATE         |
| BH-1262(800)X  |  | P.E. NUMBER |  |              |
| BRIDGE NO 5695   |  | PIN         |  | BRIDGE PLANS |
| 12628.00   |  |             |  |              |
| NEQUASSET BRIDGE<br>NEQUASSET OUTLET<br>SAGadahoc COUNTY |  | PROFILE     |  |              |
| WOOLWICH   |  |             |  |              |
| SHEET NUMBER   |  |             |  |              |
| 4  |  |             |  |              |
| OF 15  |  |             |  |              |

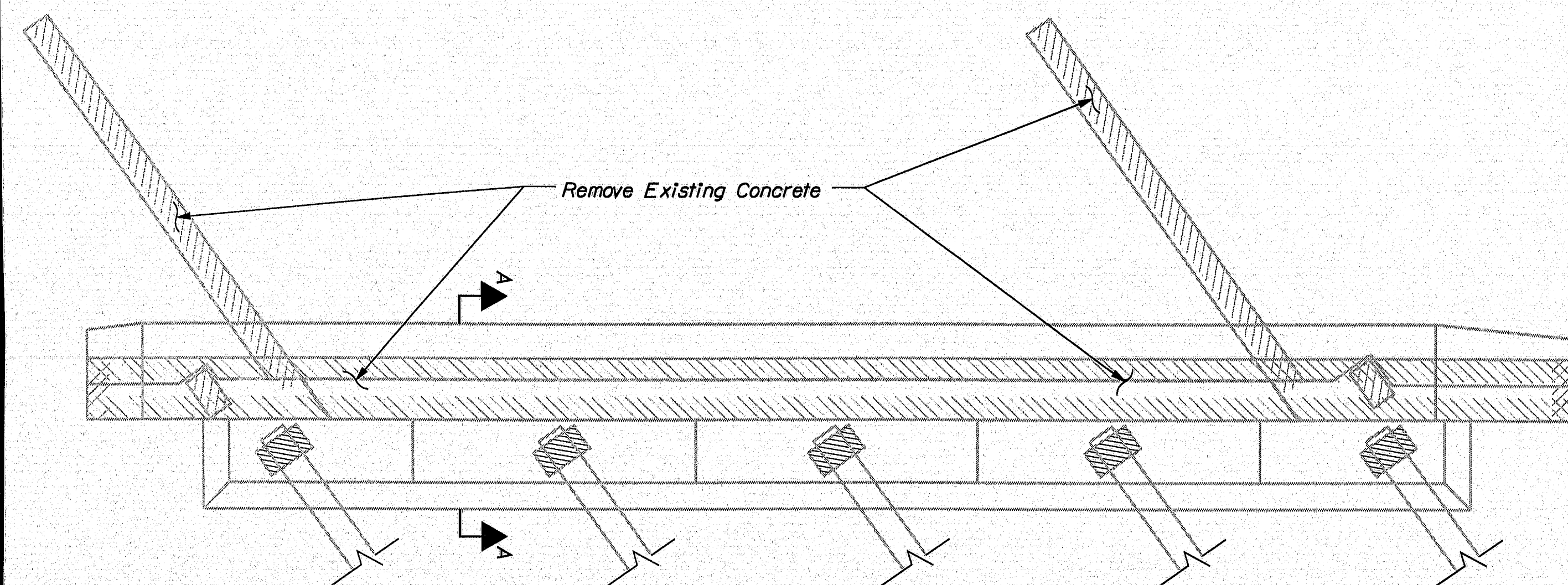


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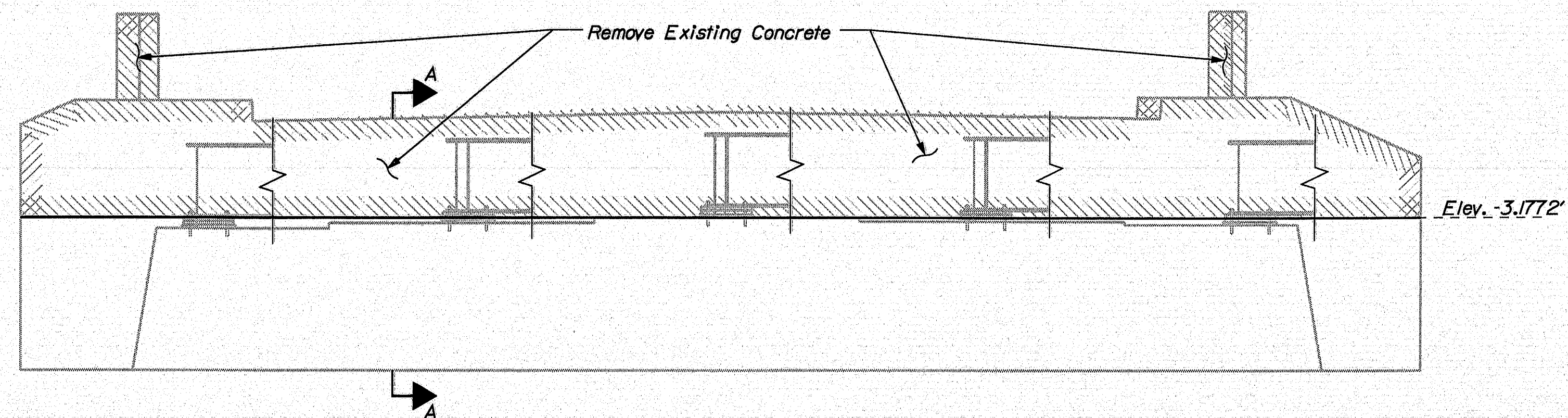
Username: david.sullivan

Division: BRIDGE

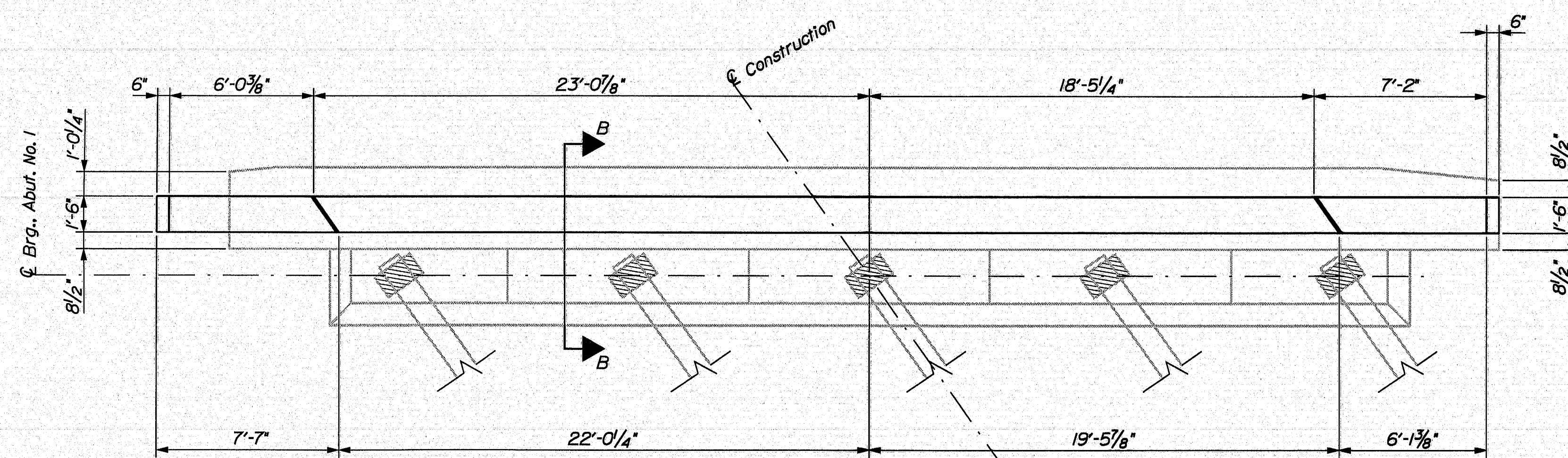
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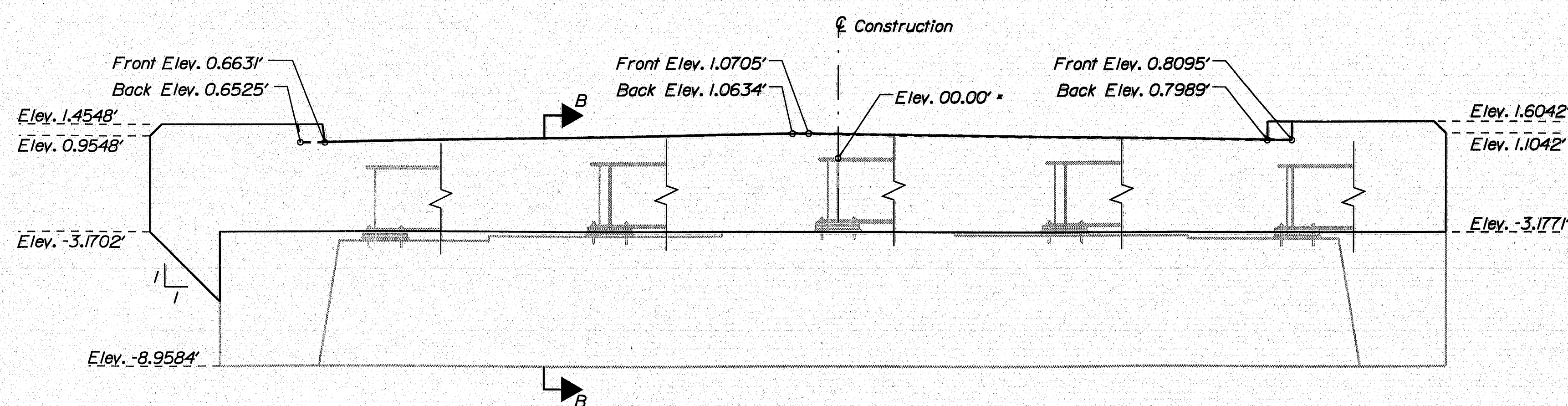
ABUTMENT No. 1  
REMOVAL OF EXISTING CONCRETE PLAN



ABUTMENT No. 1  
EXISTING CONCRETE REMOVAL ELEVATION



ABUTMENT No. 1 PLAN



ABUTMENT No. 1  
NEW CONCRETE ELEVATION

\* Bench Mark Top Of Girder  
at CL of Brg. and Girder

# ABUTMENT NOTES

1. Structural Earth Excavation, Abutments and Retaining Walls, required more than 12 inches below the bottom of the structure, will be paid for in accordance with Standard Specifications Section 206, Structural Excavation.
2. Reinforcing steel shall have a minimum concrete cover of 2 inches in walls.

|  |  |  |  |  |  |  |  |  |  |                  |  |  |  |  |  |  |  |  |  |                              |  |  |  |  |  |  |  |  |  |               |  |  |  |  |  |  |  |  |  |                 |  |  |  |  |  |  |  |  |  |              |  |  |  |  |  |  |  |  |  |              |  |  |  |  |  |  |  |  |  |      |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
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| SHEET NUMBER   |  |  |  |  |  |  |  |  |  | STATE OF MAINE   |  |  |  |  |  |  |  |  |  | DEPARTMENT OF TRANSPORTATION |  |  |  |  |  |  |  |  |  | BH-1262(800)X |  |  |  |  |  |  |  |  |  | BRIDGE NO. 5685 |  |  |  |  |  |  |  |  |  | PIN 12628.00 |  |  |  |  |  |  |  |  |  | BRIDGE PLANS |  |  |  |  |  |  |  |  |  |      |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
| WOOLWICH<br>SAGADAHOC COUNTY<br>ABUTMENT No. 1 PLANS |  |  |  |  |  |  |  |  |  | PROJ. MANAGER    |  |  |  |  |  |  |  |  |  | BEN FOSTER                   |  |  |  |  |  |  |  |  |  | BY            |  |  |  |  |  |  |  |  |  | DATE            |  |  |  |  |  |  |  |  |  | SIGNATURE    |  |  |  |  |  |  |  |  |  | P.E. NUMBER  |  |  |  |  |  |  |  |  |  | DATE |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
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|  |  |  |  |  |  |  |  |  |  | DESIGN-DETAILED3 |  |  |  |  |  |  |  |  |  |                              |  |  |  |  |  |  |  |  |  |               |  |  |  |  |  |  |  |  |  |                 |  |  |  |  |  |  |  |  |  |              |  |  |  |  |  |  |  |  |  |              |  |  |  |  |  |  |  |  |  |      |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
|  |  |  |  |  |  |  |  |  |  | REVISIONS 1      |  |  |  |  |  |  |  |  |  |                              |  |  |  |  |  |  |  |  |  |               |  |  |  |  |  |  |  |  |  |                 |  |  |  |  |  |  |  |  |  |              |  |  |  |  |  |  |  |  |  |              |  |  |  |  |  |  |  |  |  |      |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |
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|  |  |  |  |  |  |  |  |  |  | REVISIONS 4      |  |  |  |  |  |  |  |  |  |                              |  |  |  |  |  |  |  |  |  |               |  |  |  |  |  |  |  |  |  |                 |  |  |  |  |  |  |  |  |  |              |  |  |  |  |  |  |  |  |  |              |  |  |  |  |  |  |  |  |  |      |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |  |

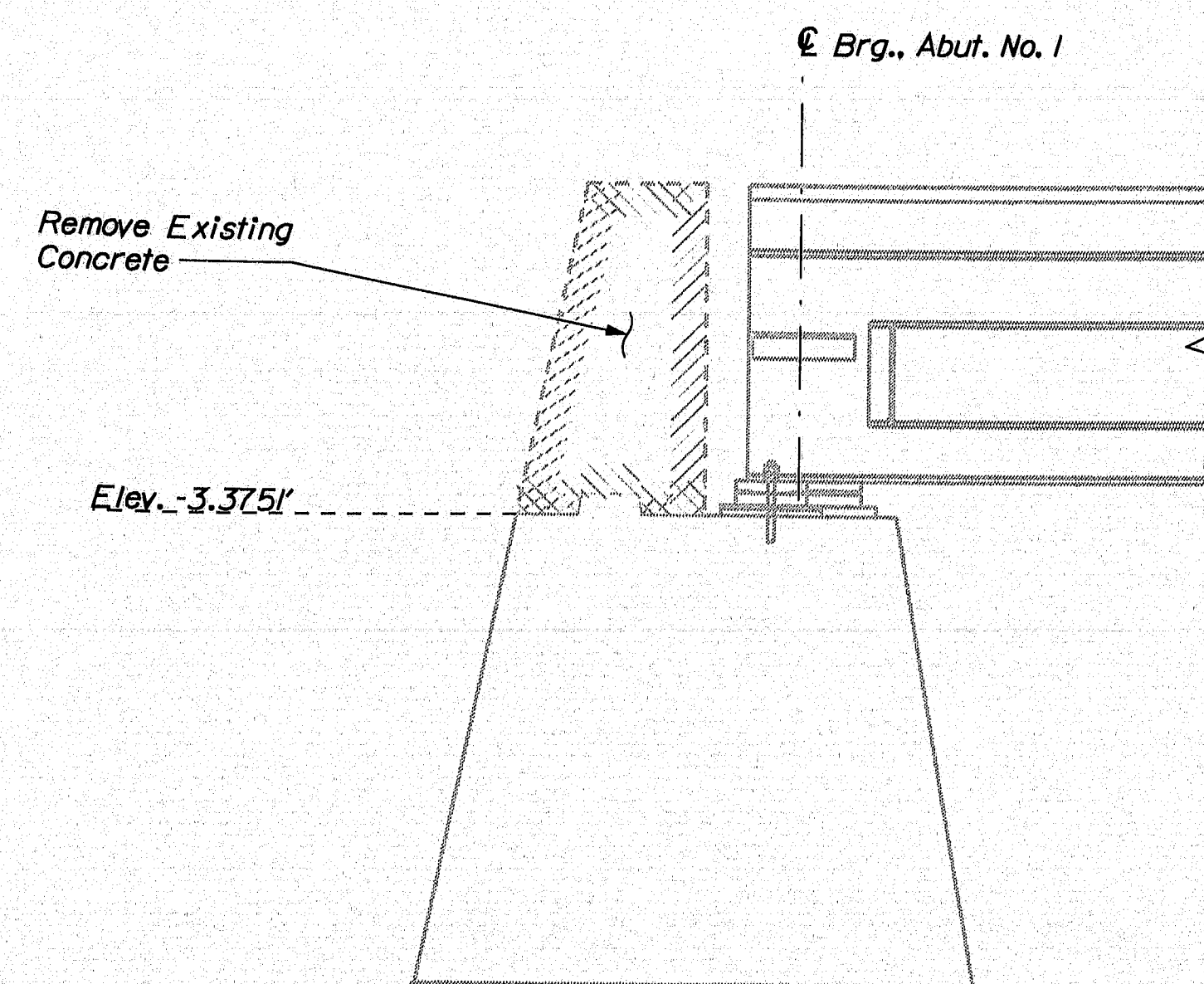


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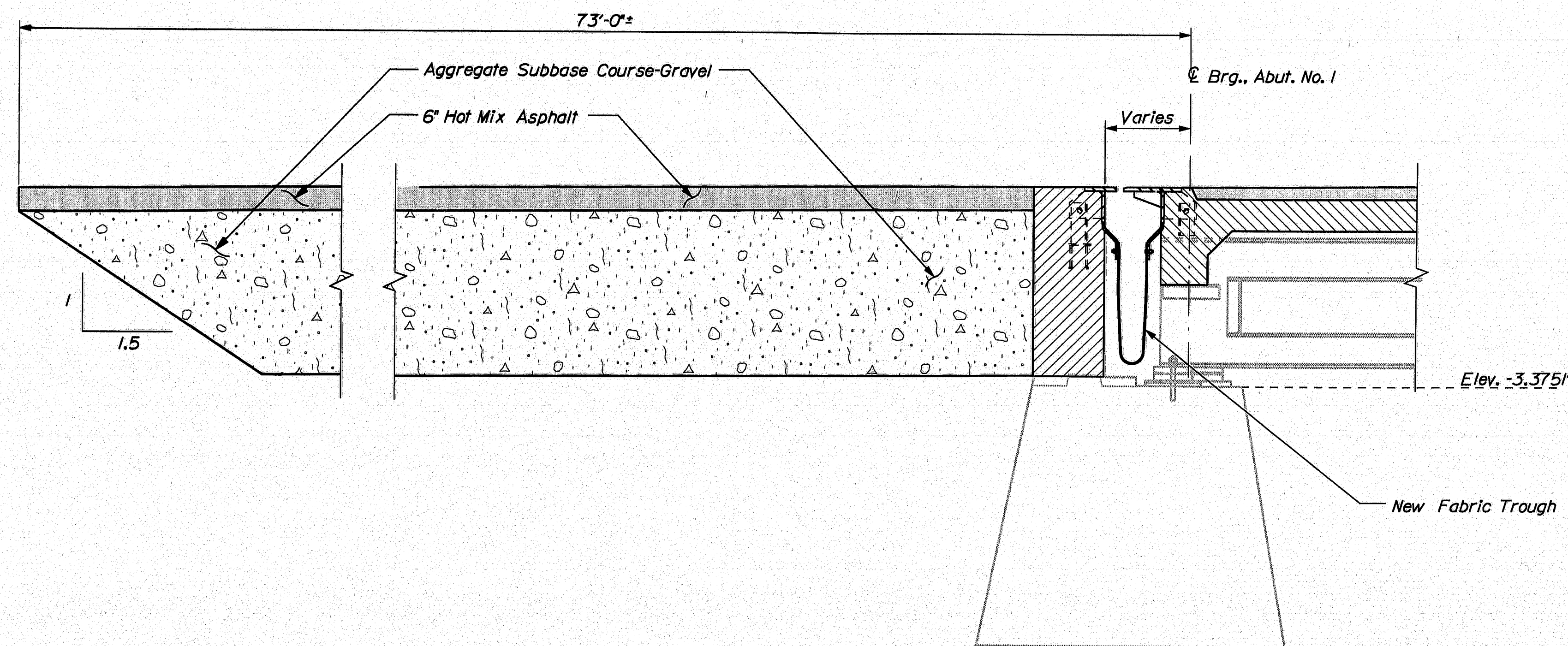
Username: david.sullivan

Division: BRIDGE

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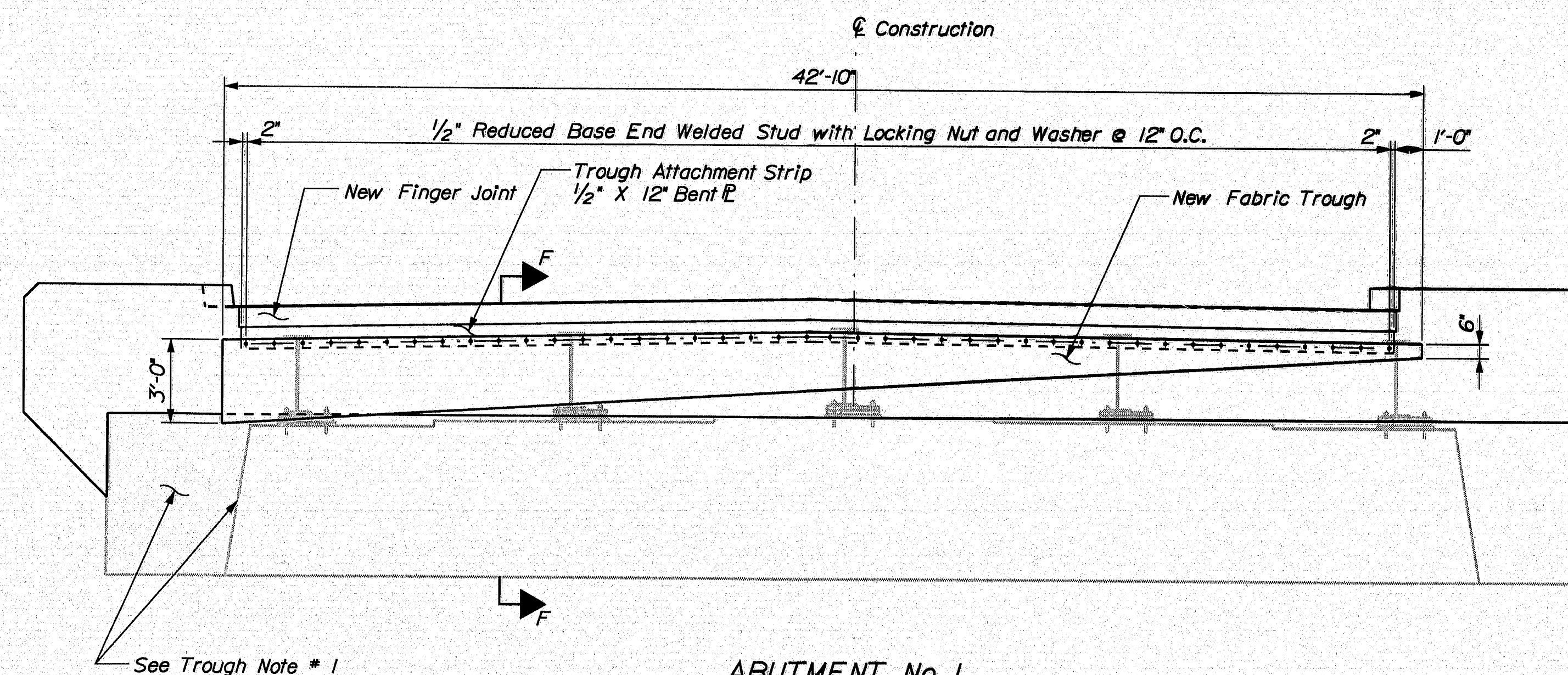


SECTION A-A  
Existing Concrete Removal



SECTION B-B  
New Concrete Placement

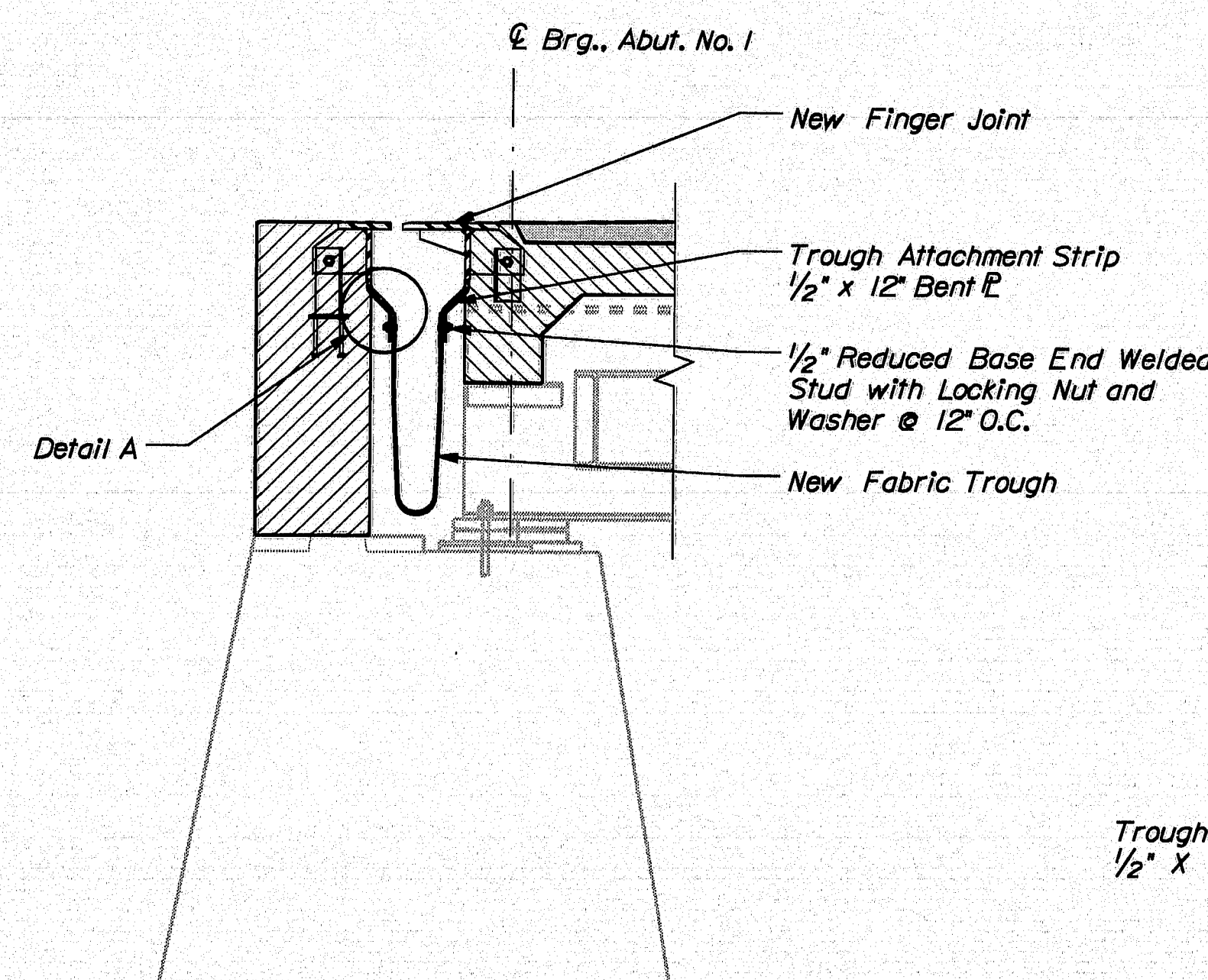
NOTE:  
Backwall and Approach Slab can be precast at Contractor's Option. Payment to be made under Item 502.219. All Precast plans must be approved by the Department.



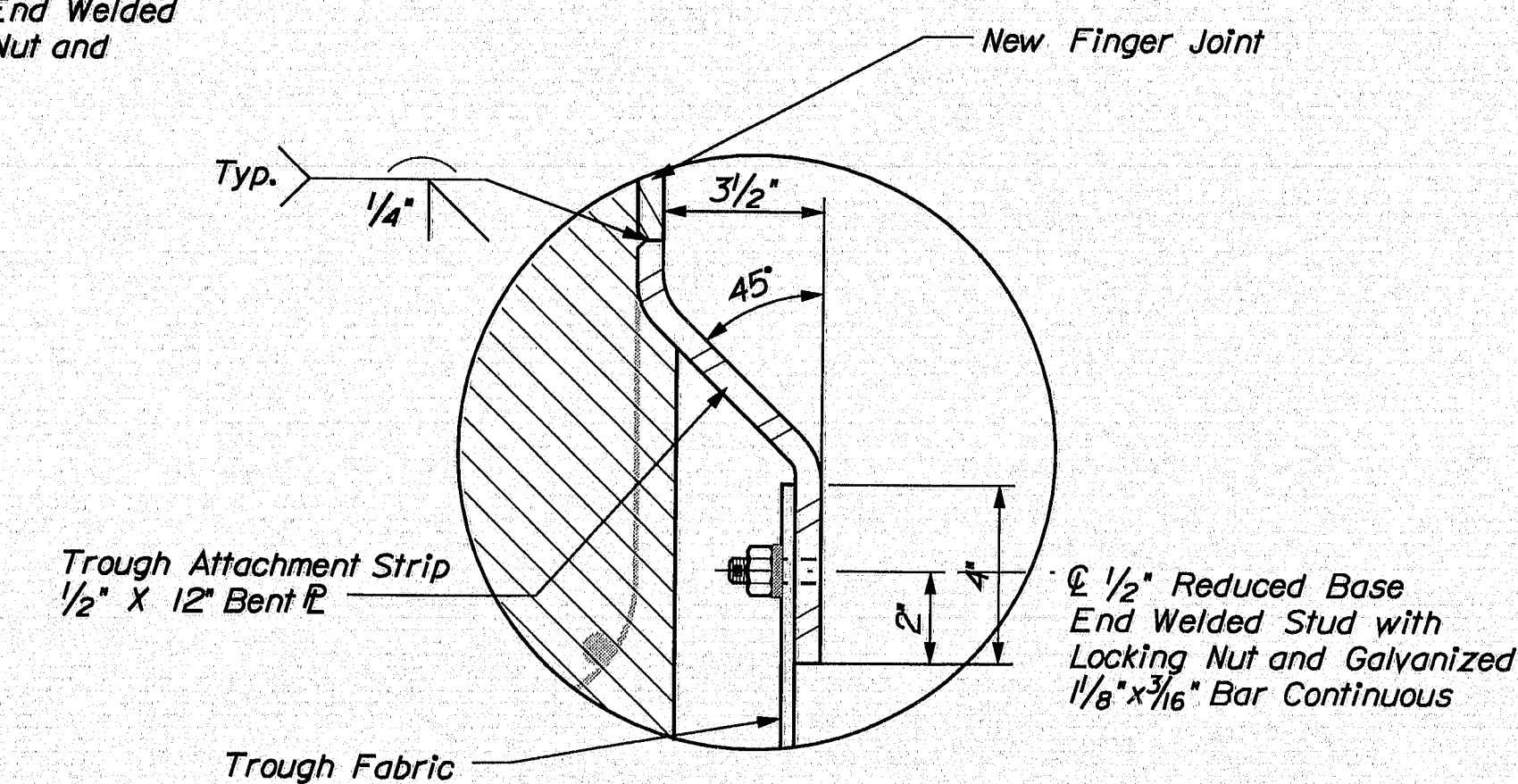
ABUTMENT No. 1  
CURTAIN TROUGH ELEVATION

TROUGH NOTES:

1. Line Concrete with Trough Material at the outlet of the Trough and as shown on the Plans. Adhere to Concrete per Trough Material Manufacturer's recommendations. Payment to be incidental to Item No. 521.32.



SECTION F-F  
Fabric Trough Placement



DETAIL A  
CURTAIN PLATE

NOTE:  
Project Benchmark - At the top of Middle Girder and the intersection of the Construction line and the Bearing of Abutment No. 1 the Elevation will be 0.00'. All Elevations on the project to be taken from this point.

|                  |  |                              |  |                  |  |                 |  |                        |  |              |  |
|------------------|--|------------------------------|--|------------------|--|-----------------|--|------------------------|--|--------------|--|
| STATE OF MAINE   |  | DEPARTMENT OF TRANSPORTATION |  | BH-1262(800)X    |  | BRIDGE NO. 5895 |  | PIN 12628.00           |  | BRIDGE PLANS |  |
| NEQUASSET BRIDGE |  | NEQUASSET OUTLET             |  | SAGadahoc COUNTY |  | WOOLWICH        |  | ABUTMENT No. 1 DETAILS |  | SHEET NUMBER |  |
| PROJ. MANAGER    |  | BEN FOSTER                   |  | BY               |  | DATE            |  | SIGNATURE              |  | P.E. NUMBER  |  |
| DESIGN-DETAILED  |  | RAC                          |  | DMS              |  |                 |  |                        |  |              |  |
| CHECKED-REVIEWED |  |                              |  |                  |  |                 |  |                        |  |              |  |
| DESIGN-DETAILED  |  |                              |  |                  |  |                 |  |                        |  |              |  |
| REVISIONS 1      |  |                              |  |                  |  |                 |  |                        |  |              |  |
| REVISIONS 2      |  |                              |  |                  |  |                 |  |                        |  |              |  |
| REVISIONS 3      |  |                              |  |                  |  |                 |  |                        |  |              |  |
| REVISIONS 4      |  |                              |  |                  |  |                 |  |                        |  |              |  |
| FIELD CHANGES    |  |                              |  |                  |  |                 |  |                        |  |              |  |

6

OF 15





SECTION D-D

SECTION E-E

*New Concrete Placement*

|              |       |   |  |                 |            |     |      |  |               |             |      |
|--------------|-------|---|--|-----------------|------------|-----|------|--|---------------|-------------|------|
| SHEET NUMBER |       | NEQUASSET BRIDGE<br>NEQUASSET OUTLET<br>WOOLWICH SAGADAHOC COUNTY |  | PROJ. MANAGER   | BEN FOSTER | BY  | DATE | STATE OF MAINE<br>DEPARTMENT OF TRANSPORTATION |               |             |      |
| 2            | OF 15 | ABUTMENT No. 1 REINFORCING STEEL                                  | DESIGN-DETAILED<br>CHECKED-REVIEWED<br>DESIGN-DETAILED<br>DESIGN-DETAILED<br>REVISIONS 1<br>REVISIONS 2<br>REVISIONS 3<br>REVISIONS 4<br>FIELD CHANGES | DESIGN-DETAILED | IRC        | DMS |      | SIGNATURE                                      | BH-1262(800)X |             |      |
|              |       |   |  | DESIGN-DETAILED |            |     |      |  |               | P.L. NUMBER |      |
|              |       |   |  | DESIGN-DETAILED |            |     |      |  |               |             |      |
|              |       |   |  | REVISIONS 1     |            |     |      |  |               |             | DATE |
|              |       |   |  | REVISIONS 2     |            |     |      |  |               |             |      |
|              |       |   |  | REVISIONS 3     |            |     |      |  |               |             |      |
| REVISIONS 4  |       |   |  |                 |            |     |      |  |               |             |      |
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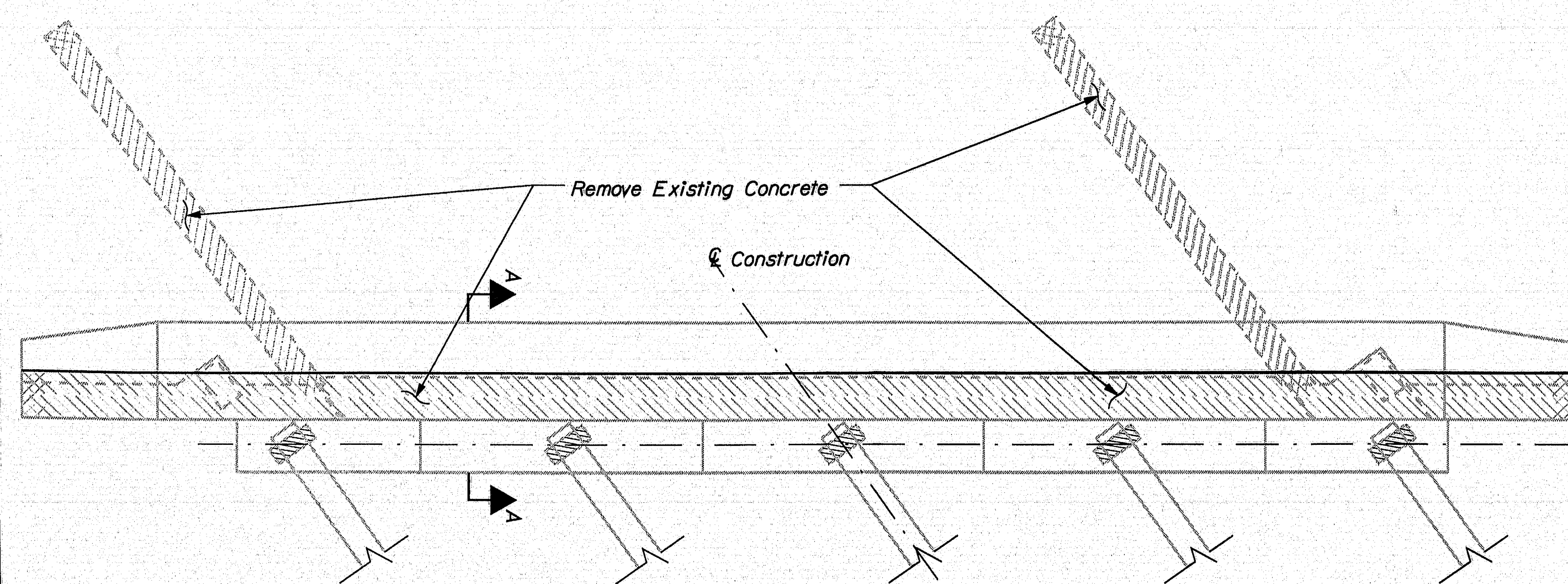


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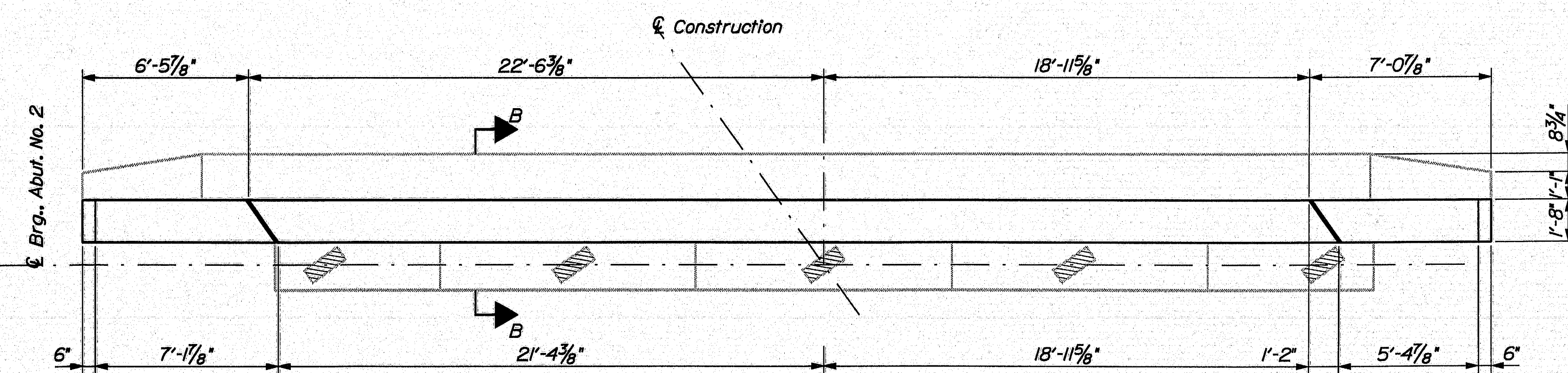
Username: david.sullivan

Division: BRIDGE

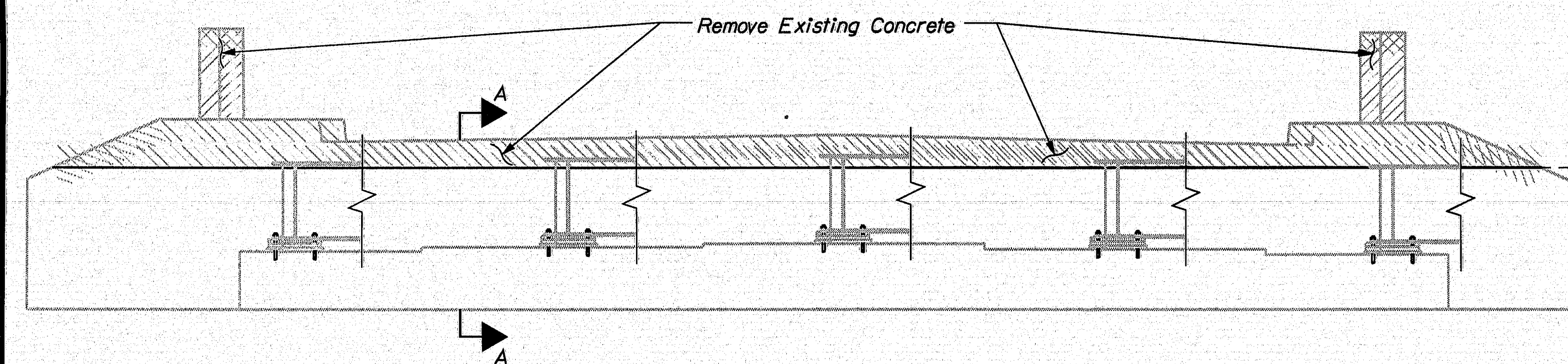
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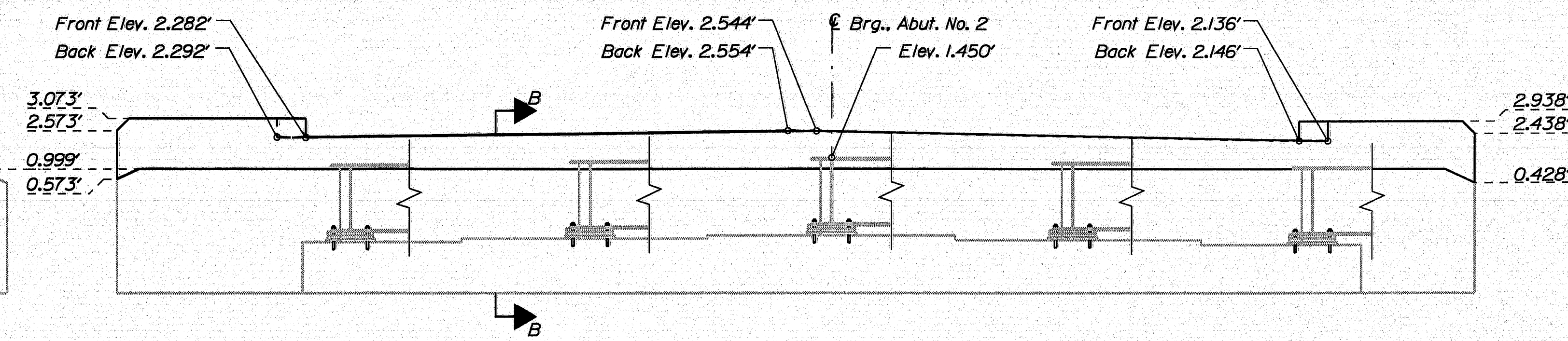
ABUTMENT No. 2  
REMOVAL OF EXISTING CONCRETE PLAN



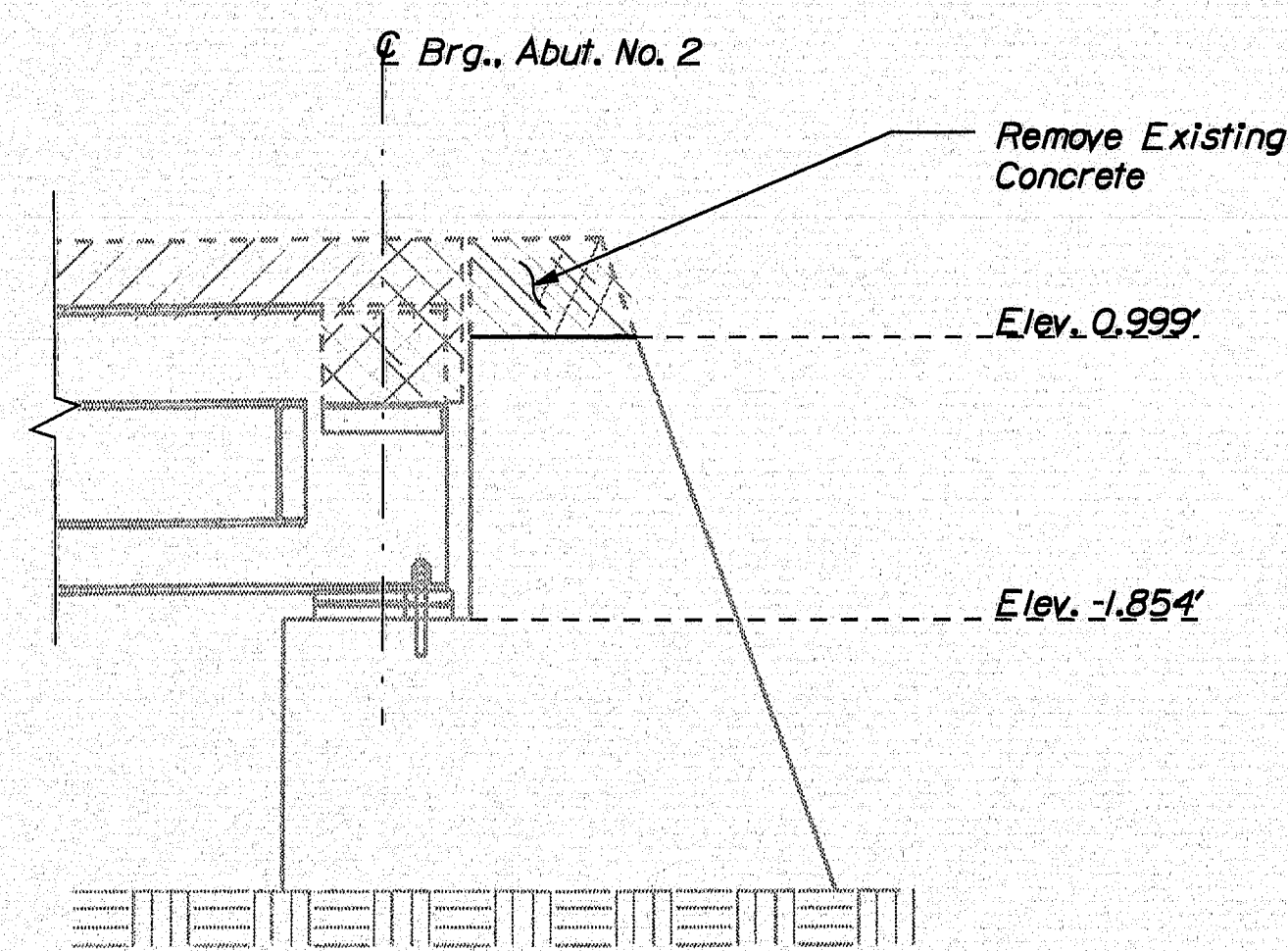
ABUTMENT No. 2 PLAN



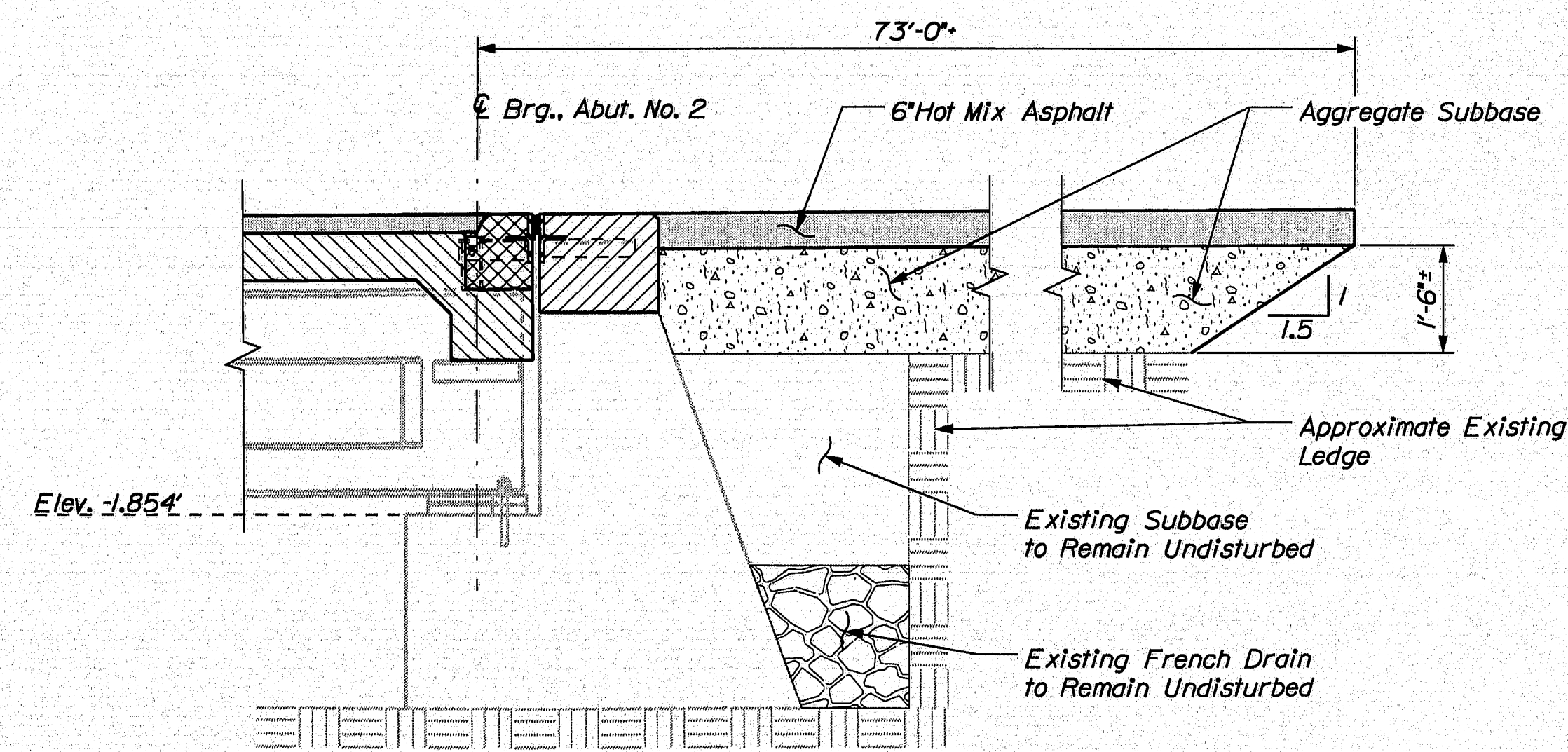
ABUTMENT No. 2  
EXISTING CONCRETE REMOVAL ELEVATION



ABUTMENT No. 2  
NEW CONCRETE ELEVATION



SECTION A-A  
Existing Concrete Removal

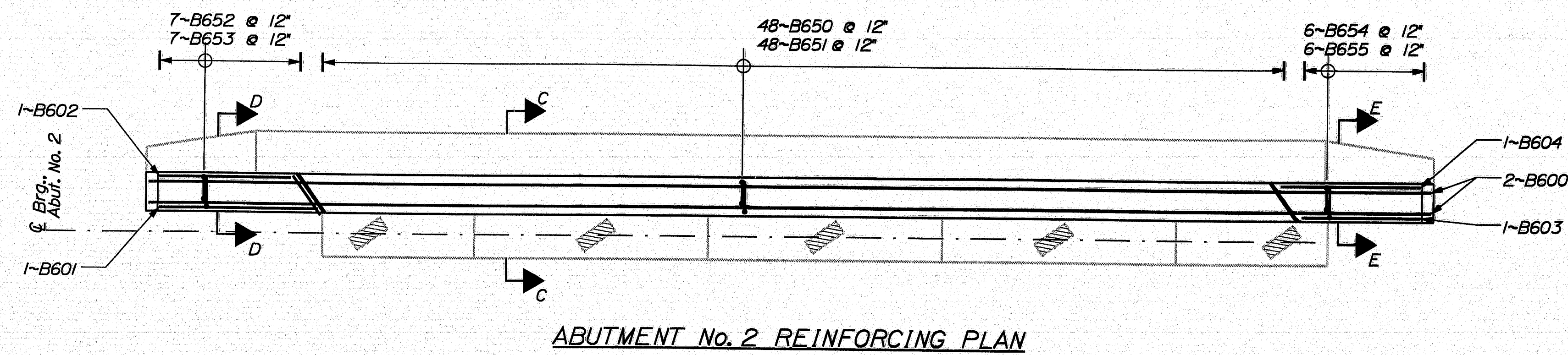


SECTION B-B  
New Concrete Placement

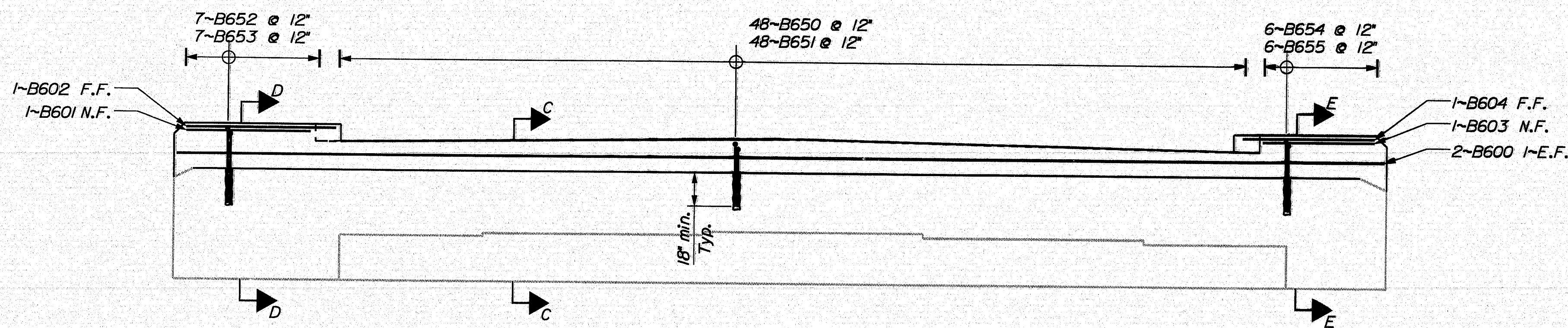
NOTE:  
Project Benchmark - At the top of Middle Girder and the intersection  
of the Construction line and the Bearing of Abutment No. 1 the  
Elevation will be 0.00'. All Elevations on the project to be taken from  
this point.

|                  |  |                              |  |                  |  |                  |  |                      |  |               |  |
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| STATE OF MAINE   |  | DEPARTMENT OF TRANSPORTATION |  | BH-1262(800)X    |  | PIN 12628.00     |  | BRIDGE NO. 5695      |  | BRIDGE PLANS  |  |
| NEQUASSET BRIDGE |  | NEQUASSET OUTLET             |  | SAGadahoc COUNTY |  | WOOLWICH         |  | ABUTMENT No. 2 PLANS |  | SHEET NUMBER  |  |
| PROJ. MANAGER    |  | DESIGN-DETAILED              |  | CHECKED-REVIEWED |  | DESIGN-DETAILED2 |  | REVISIONS 1          |  | REVISIONS 2   |  |
| BY               |  | DATE                         |  | SIGNATURE        |  | P.L. NUMBER      |  | DATE                 |  | FIELD CHANGES |  |
| 8                |  | 15                           |  | 145-333          |  |                  |  |                      |  |               |  |

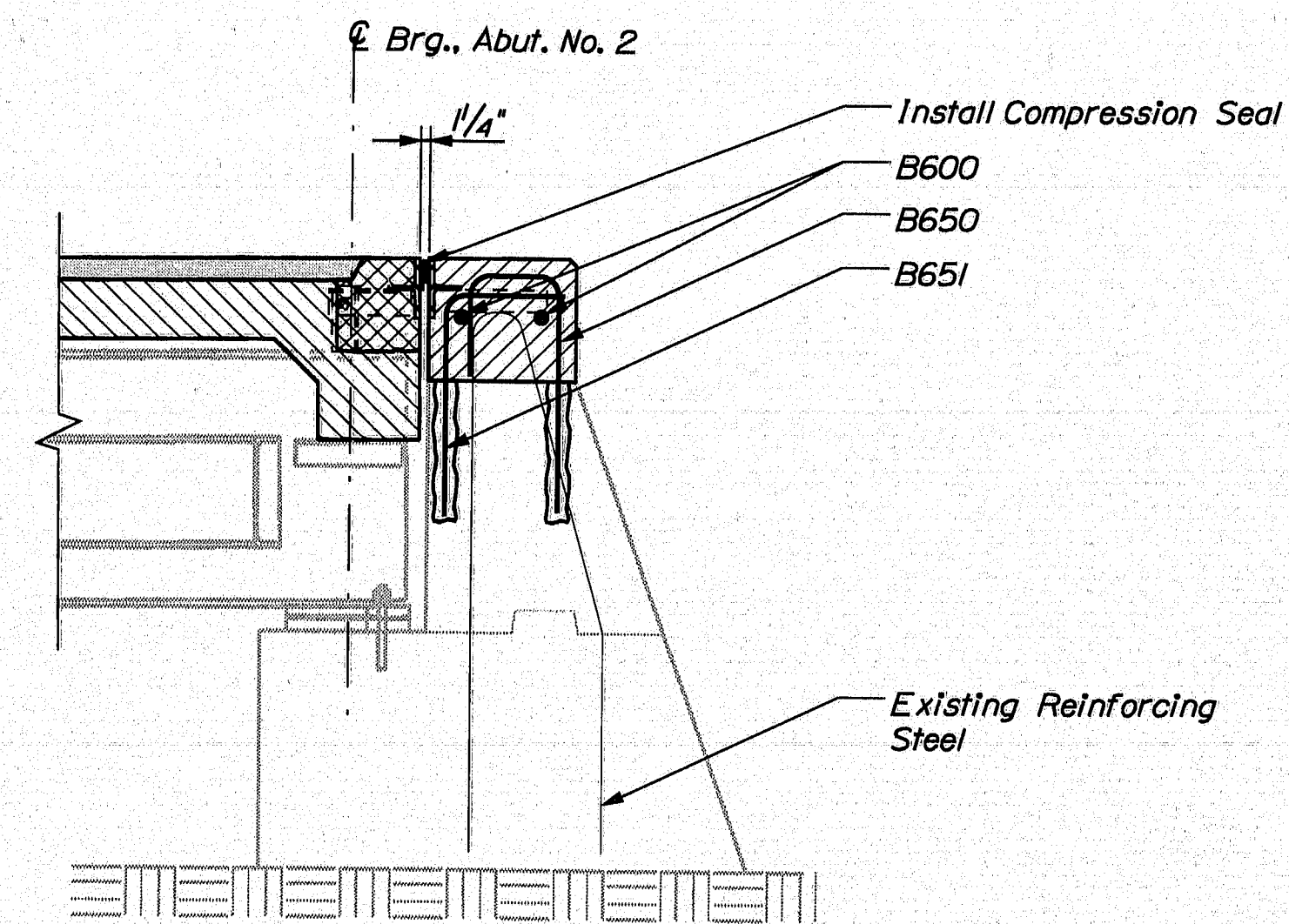




ABUTMENT No. 2 REINFORCING PLAN

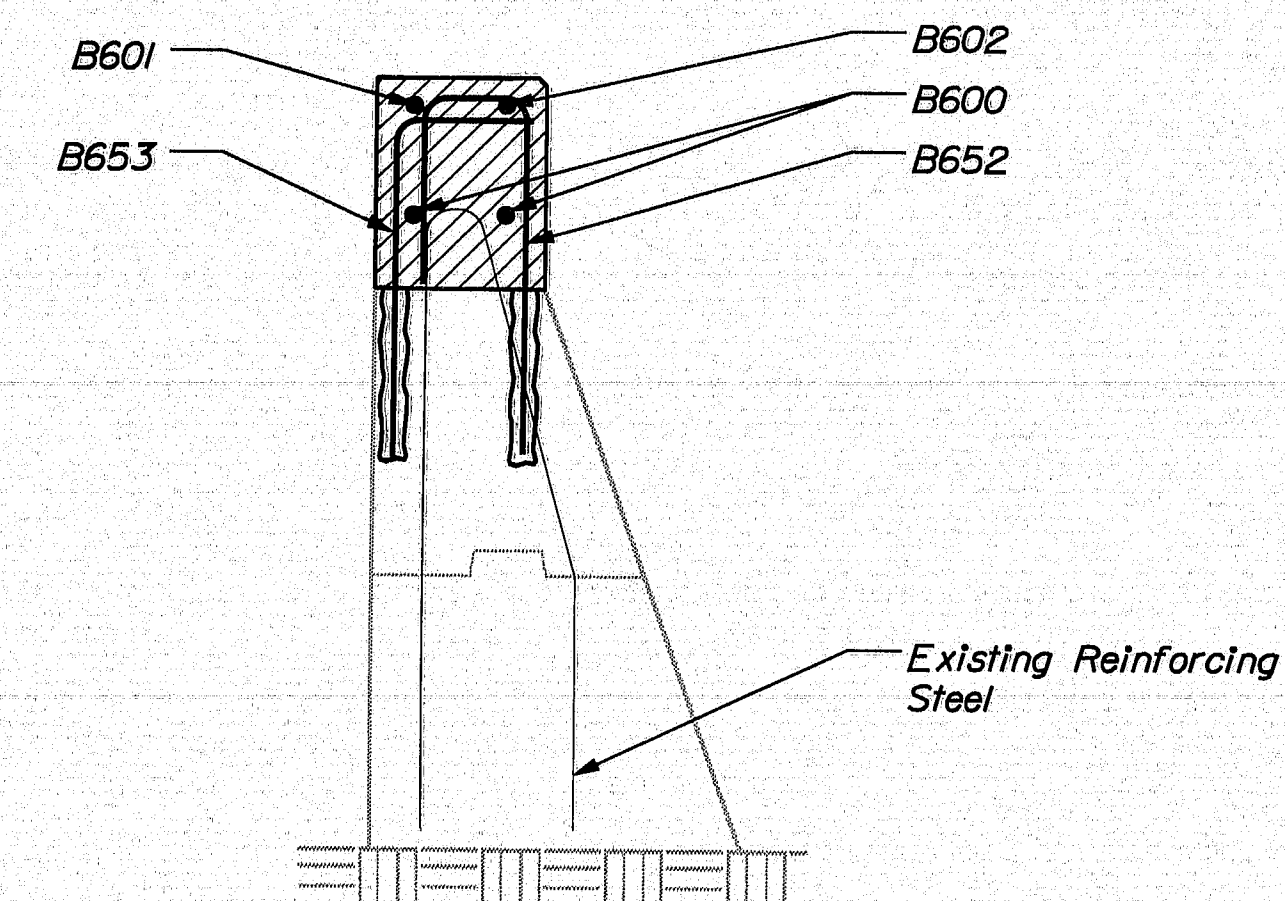


ABUTMENT No. 2 REINFORCING STEEL ELEVATION



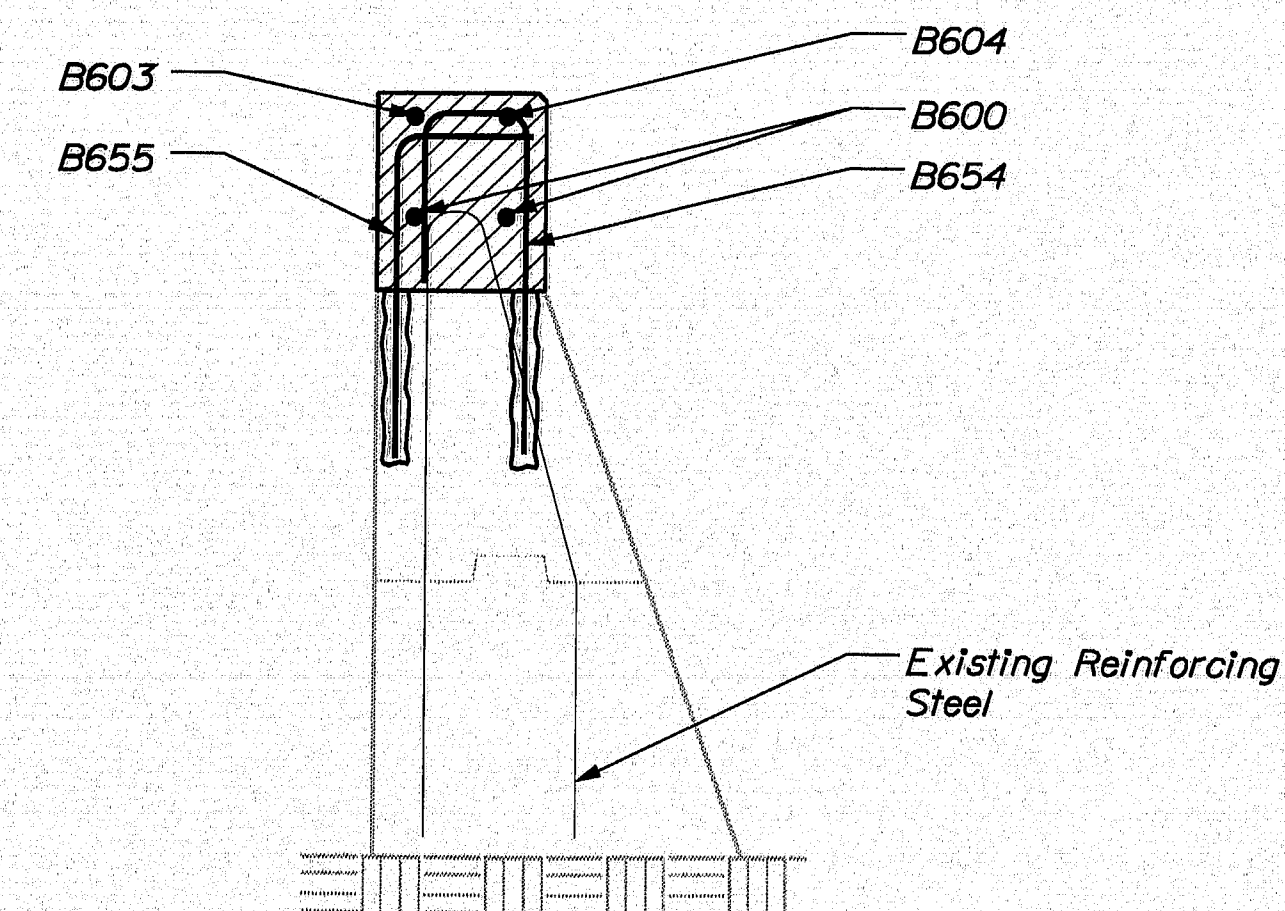
SECTION C-C

New Concrete Placement



SECTION D-D

New Concrete Placement



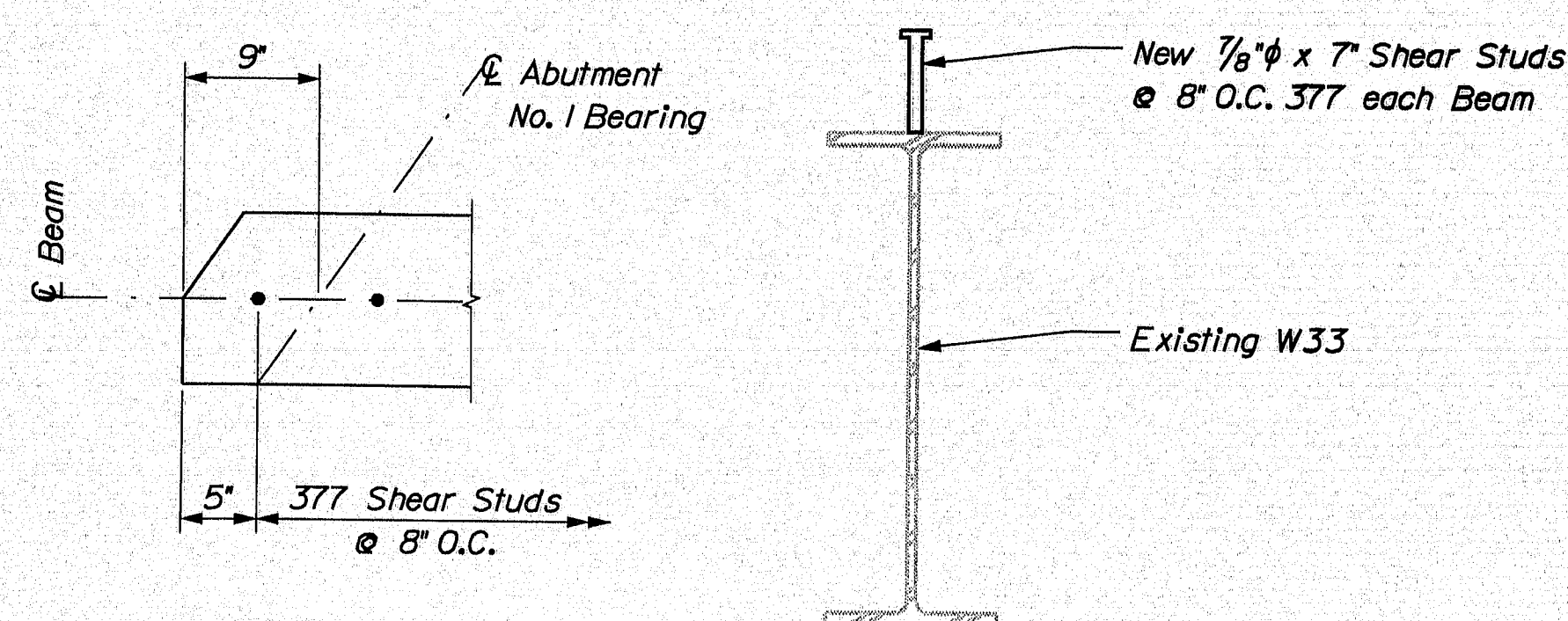
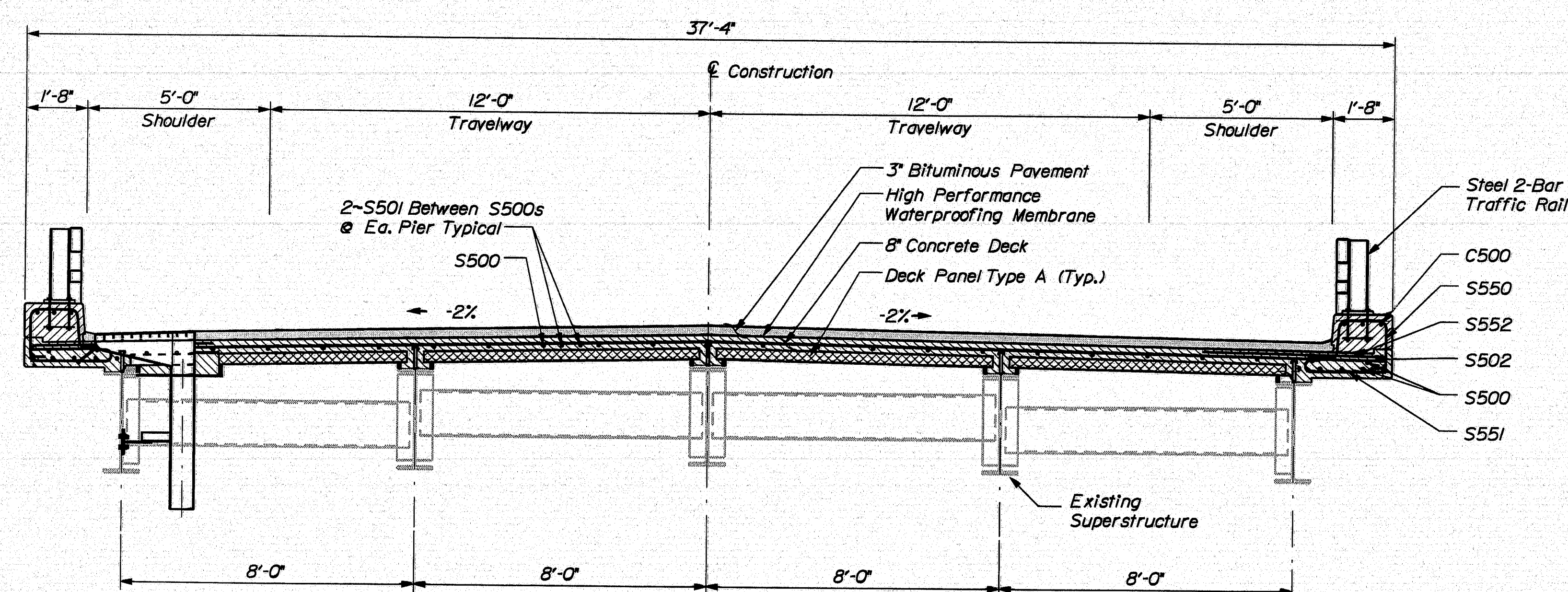
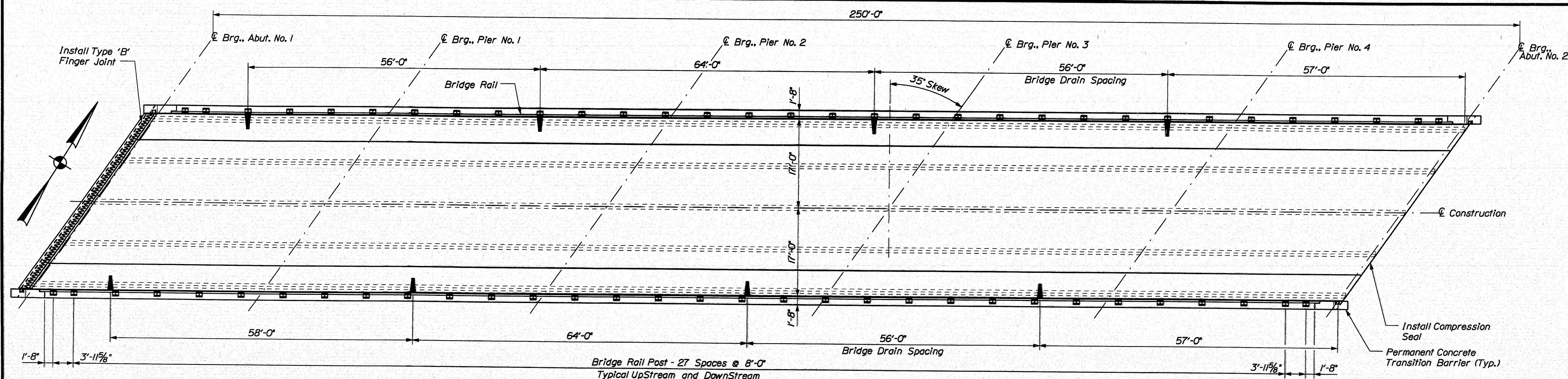
SECTION E-E

New Concrete Placement

145-334

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| STATE OF MAINE<br>DEPARTMENT OF TRANSPORTATION<br>BH-1262(800)X  | SHEET NUMBER     |    | 9            |             |
|  | BRIDGE NO. 5695  |    | PIN 12628.00 |             |
|  | BRIDGE PLANS     |    | DATE         |             |
| NEQUASSET BRIDGE<br>NEQUASSET OUTLET<br>WOOLWICH<br>SAGadahoc COUNTY<br>ABUTMENT No. 2 REINFORCING STEEL | PROJ. MANAGER    | BY | DATE         | SIGNATURE   |
|  | DESIGN-DETAILED  | RC | DATE         | P.E. NUMBER |
|  | CHECKED-REVIEWED | RC | DATE         | DATE        |
|  | DESIGN-DETAILED  | RC | DATE         | DATE        |
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| REVISIONS 2  |                  |    |              |             |
| REVISIONS 3  |                  |    |              |             |
| REVISIONS 4  |                  |    |              |             |
| FIELD CHANGES  |                  |    |              |             |





## SUPERSTRUCTURE NOTES

1. The theoretical blocking used for design of the structure is 2 inches at the centerline of bearing at the abutments and piers. Refer to Standard Detail 502(02) for blocking details.
2. Reinforcing steel shall have a minimum concrete cover of 2 inches unless otherwise noted.
3. Adjunct reinforcing steel to fit around the bridge drains in a manner approved by the Resident. Do not cut transverse reinforcing bars.
4. Form a one inch V-groove on the fascias at the horizontal joint between the curb and slab.
5. Unless the superstructure slab concrete is placed in one continuous operation, the initial placement shall begin at a simply supported end of the deck slab and shall terminate at the completion of a positive moment section. Successive placements shall proceed from the end of the previous placement, terminate at the completion of a positive moment section, and include two or more spans. Concrete in a placement shall be kept plastic one complete span behind the span being placed. A minimum of 5 days shall elapse between successive partial placements. The superstructure slab concrete placement sequence shall be approved by the Resident.
6. Precast Deck Panels shall be used in accordance with Special Provisions Section 502, Structural Concrete - Precast Deck Panels, and in accordance with the Standard Details.
7. Payment for reinforcing steel fabricated, delivered, and placed in the cast-in-place portion of the structural concrete slab will be considered incidental to the appropriate Section 502 pay item.
8. The Compression Seal to be furnished shall have minimum Movement Rating as follows:  
  
Abutment No. 2 = 1/2 inch
9. The Compression Seal installation width shall be 1 inch.
10. The Resident shall approve the seals prior to fabrication of the Expansion Device.
11. The Contractor shall install Transition Barrier vertical closed stirrups, as shown in Standard Details Section 526, prior to the placement of the curb or sidewalk concrete.
12. All Items associated with the Steel Traffic Rail Modification as Detailed shall be paid for under Item No. 507.08(1) Steel Bridge Railing, 2 Bar.
13. Deck Panels will have 21-Strands equally spaced. See Standard Detail 502(08).

145-335

|  |                        |
|--|------------------------|
| STATE OF MAINE<br>DEPARTMENT OF TRANSPORTATION |                        |
| <b>BH-1262(800)X</b>                           |                        |
| BRIDGE NO. 5695                                | PIN<br><b>12628.00</b> |
|  | BRIDGE PLANS           |

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| SIGNATURE   |
| P.E. NUMBER |
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| PROJ. MANAGER     | BEN FOSTER | BY  | DATE |
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| DESIGN3-DETAILED3 |            |     |      |
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| REVISIONS 2       |            |     |      |
| REVISIONS 3       |            |     |      |
| REVISIONS 4       |            |     |      |

WOOLWICH      NEQUASSET BRIDGE  
NEQUASSET OUTLET      SAGadahoc COUNTY

**SUPERSTRUCTURE PLAN**

SHEET NUMBER

10

OF 15

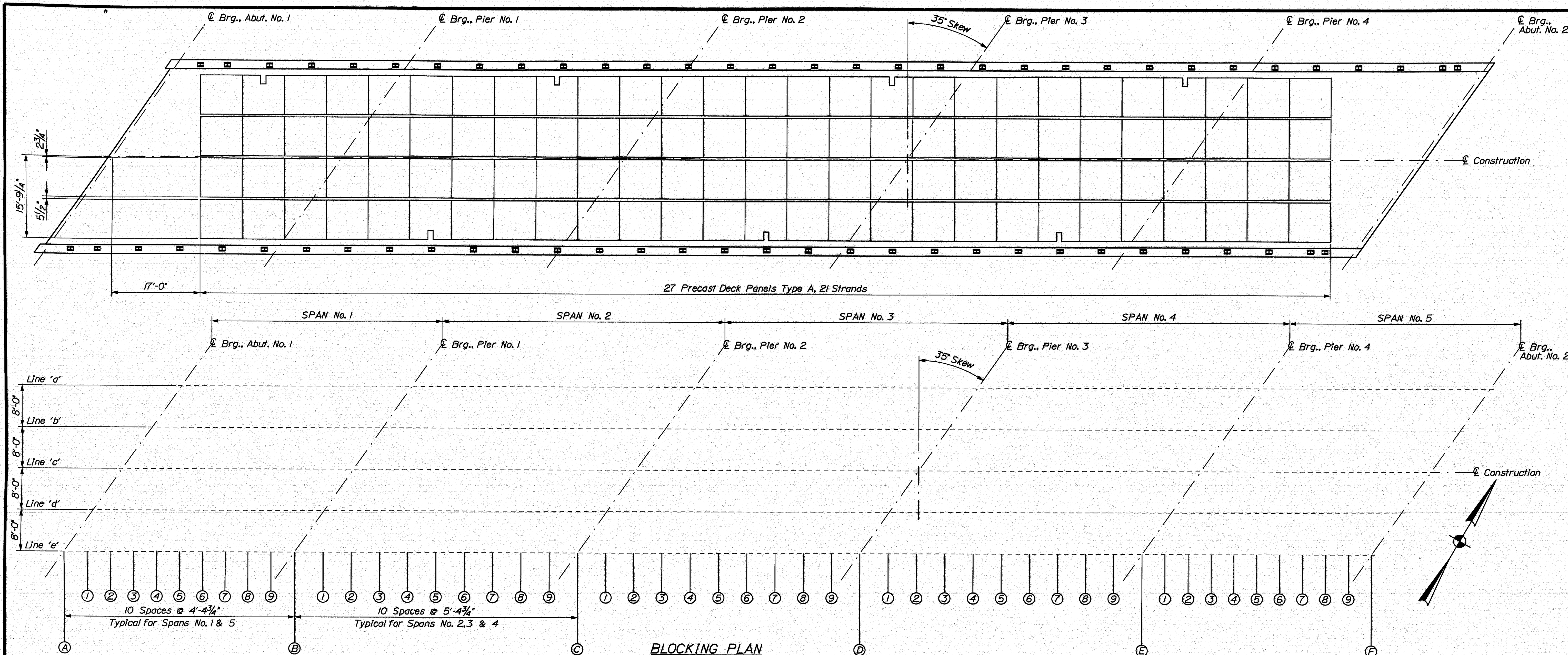


Date: 11/7/2007

Username: david.sullivan

Division: BRIDGE

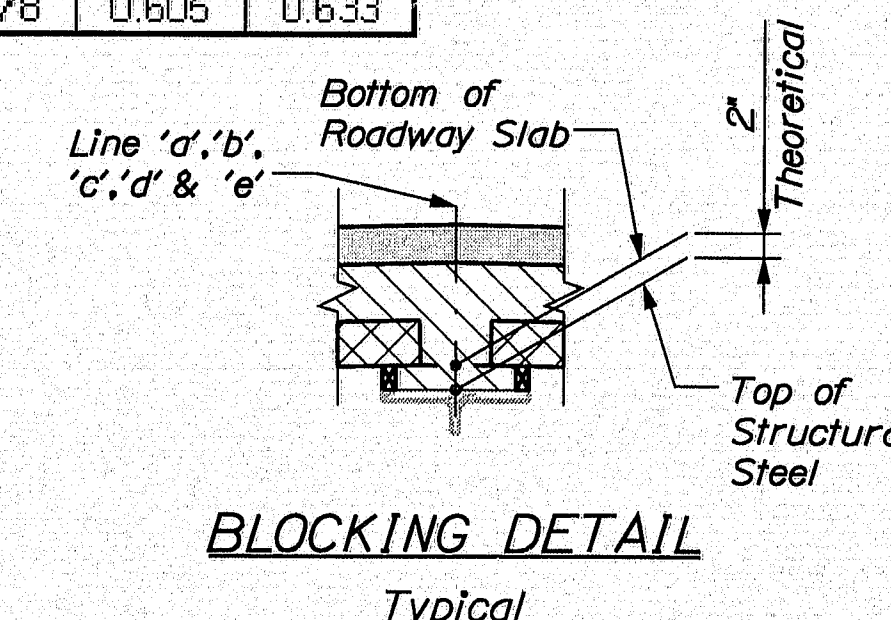
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BLOCKING PLAN

| ELEVATIONS ~ BOTTOM OF ROADWAY SLAB (in) |        |        |        |        |        |        |        |        |        |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |
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| Point                                    | A      | A1     | A2     | A3     | A4     | A5     | A6     | A7     | A8     | A9    | B     | B1    | B2    | B3    | B4    | B5    | B6    | B7    | BB    | B9    | C     | C1    | C2    | C3    | C4    | C5    | C6    | C7    | C8    | C9    |
| Line 'a'                                 | -0.089 | -0.059 | -0.029 | 0.000  | 0.027  | 0.052  | 0.075  | 0.098  | 0.119  | 0.141 | 0.167 | 0.200 | 0.235 | 0.271 | 0.305 | 0.337 | 0.368 | 0.396 | 0.423 | 0.450 | 0.480 | 0.513 | 0.547 | 0.583 | 0.617 | 0.650 | 0.680 | 0.708 | 0.735 | 0.763 |
| Line 'b'                                 | 0.039  | 0.069  | 0.099  | 0.128  | 0.155  | 0.180  | 0.203  | 0.226  | 0.247  | 0.269 | 0.295 | 0.328 | 0.363 | 0.399 | 0.433 | 0.465 | 0.496 | 0.524 | 0.551 | 0.578 | 0.608 | 0.641 | 0.675 | 0.711 | 0.745 | 0.778 | 0.808 | 0.836 | 0.863 | 0.891 |
| Line 'c'                                 | 0.167  | 0.197  | 0.227  | 0.256  | 0.283  | 0.308  | 0.331  | 0.354  | 0.375  | 0.397 | 0.422 | 0.455 | 0.490 | 0.526 | 0.560 | 0.592 | 0.623 | 0.651 | 0.678 | 0.705 | 0.735 | 0.768 | 0.802 | 0.838 | 0.872 | 0.905 | 0.935 | 0.963 | 0.990 | 1.018 |
| Line 'd'                                 | -0.026 | 0.004  | 0.034  | 0.063  | 0.090  | 0.115  | 0.138  | 0.161  | 0.182  | 0.204 | 0.229 | 0.262 | 0.297 | 0.333 | 0.367 | 0.399 | 0.430 | 0.458 | 0.485 | 0.512 | 0.542 | 0.575 | 0.609 | 0.645 | 0.679 | 0.712 | 0.742 | 0.770 | 0.797 | 0.825 |
| Line 'e'                                 | -0.218 | -0.188 | -0.158 | -0.129 | -0.102 | -0.077 | -0.054 | -0.031 | -0.010 | 0.012 | 0.037 | 0.070 | 0.105 | 0.141 | 0.175 | 0.207 | 0.238 | 0.266 | 0.293 | 0.320 | 0.350 | 0.383 | 0.417 | 0.453 | 0.487 | 0.520 | 0.550 | 0.578 | 0.605 | 0.633 |

| ELEVATIONS ~ BOTTOM OF ROADWAY SLAB (in) |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |       |
|--|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| Point                                    | D     | D1    | D2    | D3    | D4    | D5    | D6    | D7    | D8    | D9    | E     | E1    | E2    | E3    | E4    | E5    | E6    | E7    |
| Line 'a'                                 | 0.793 | 0.826 | 0.861 | 0.897 | 0.931 | 0.963 | 0.994 | 1.022 | 1.049 | 1.076 | 1.106 | 1.132 | 1.161 | 1.191 | 1.219 | 1.247 | 1.273 | 1.297 |
| Line 'b'                                 | 0.921 | 0.954 | 0.989 | 1.025 | 1.059 | 1.091 | 1.122 | 1.150 | 1.177 | 1.204 | 1.234 | 1.260 | 1.289 | 1.319 | 1.347 | 1.375 | 1.401 | 1.425 |
| Line 'c'                                 | 1.048 | 1.081 | 1.116 | 1.152 | 1.186 | 1.218 | 1.249 | 1.277 | 1.304 | 1.331 | 1.361 | 1.387 | 1.416 | 1.446 | 1.474 | 1.502 | 1.528 | 1.552 |
| Line 'd'                                 | 0.855 | 0.888 | 0.923 | 0.959 | 0.993 | 1.025 | 1.056 | 1.084 | 1.111 | 1.138 | 1.168 | 1.194 | 1.223 | 1.253 | 1.281 | 1.309 | 1.335 | 1.359 |
| Line 'e'                                 | 0.663 | 0.696 | 0.731 | 0.767 | 0.801 | 0.833 | 0.864 | 0.892 | 0.919 | 0.946 | 0.976 | 1.002 | 1.031 | 1.061 | 1.089 | 1.117 | 1.143 | 1.167 |



BLOCKING DETAIL  
Typical

| TABLE OF DEFLECTION |                |      |      |      |      |      |      |      |      |      |            |      |      |      |      |      |      |      |      |      |            |      |      |      |      |      |      |      |      |      |
|---------------------|----------------|------|------|------|------|------|------|------|------|------|------------|------|------|------|------|------|------|------|------|------|------------|------|------|------|------|------|------|------|------|------|
| Dead Load/Point     | Abutment No. 1 |      |      |      |      |      |      |      |      |      | Pier No. 1 |      |      |      |      |      |      |      |      |      | Pier No. 2 |      |      |      |      |      |      |      |      |      |
|                     | A              | A1   | A2   | A3   | A4   | A5   | A6   | A7   | A8   | A9   | B          | B1   | B2   | B3   | B4   | B5   | B6   | B7   | B8   | B9   | C          | C1   | C2   | C3   | C4   | C5   | C6   | C7   | C8   | C9   |
| Fluid               | 0              | 0.06 | 0.1  | 0.14 | 0.15 | 0.15 | 0.12 | 0.09 | 0.05 | 0.02 | 0          | 0.02 | 0.06 | 0.11 | 0.14 | 0.15 | 0.14 | 0.1  | 0.06 | 0.02 | 0          | 0.01 | 0.04 | 0.08 | 0.11 | 0.12 | 0.11 | 0.08 | 0.04 | 0.01 |
| Superimposed        | 0              | 0.01 | 0.02 | 0.03 | 0.03 | 0.03 | 0.02 | 0.02 | 0.01 | 0    | 0          | 0    | 0.01 | 0.02 | 0.03 | 0.03 | 0.03 | 0.02 | 0.01 | 0    | 0          | 0    | 0.01 | 0.02 | 0.03 | 0.03 | 0.03 | 0.02 | 0.01 | 0    |
| Total               | 0              | 0.07 | 0.12 | 0.17 | 0.18 | 0.18 | 0.14 | 0.11 | 0.06 | 0.02 | 0          | 0.02 | 0.07 | 0.13 | 0.17 | 0.18 | 0.17 | 0.12 | 0.07 | 0.02 | 0          | 0.01 | 0.05 | 0.1  | 0.14 | 0.15 | 0.14 | 0.1  | 0.05 | 0.01 |

| TABLE OF DEFLECTION |            |      |      |      |      |      |      |      |      |      |            |      |      |      |      |      |      |      |
|---------------------|------------|------|------|------|------|------|------|------|------|------|------------|------|------|------|------|------|------|------|
| Dead Load/Point     | Pier No. 3 |      |      |      |      |      |      |      |      |      | Pier No. 4 |      |      |      |      |      |      |      |
|                     | D          | D1   | D2   | D3   | D4   | D5   | D6   | D7   | D8   | D9   | E          | E1   | E2   | E3   | E4   | E5   | E6   | E7   |
| Fluid               | 0          | 0.02 | 0.06 | 0.1  | 0.14 | 0.15 | 0.14 | 0.11 | 0.06 | 0.02 | 0          | 0.02 | 0.06 | 0.09 | 0.12 | 0.15 | 0.15 | 0.14 |
| Superimposed        | 0          | 0    | 0.01 | 0.02 | 0.03 | 0.03 | 0.03 | 0.02 | 0.01 | 0    | 0          | 0    | 0.01 | 0.02 | 0.02 | 0.03 | 0.03 | 0.03 |
| Total               | 0          | 0.02 | 0.07 | 0.12 | 0.17 | 0.18 | 0.17 | 0.13 | 0.07 | 0.02 | 0          | 0.02 | 0.06 | 0.11 | 0.14 | 0.18 | 0.18 | 0.17 |

145-336

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION  
BH-1262(800)X  
BRIDGE NO. 5995  
PIN 12628.00  
BRIDGE PLANS

PROJ. MANAGER  
DESIGN-DETAILED  
CHECKED-REVIEWED  
DESIGN-DETAILED  
DESIGN-DETAILED  
REVISIONS 1  
REVISIONS 2  
REVISIONS 3  
FIELD CHANGES  
DATE  
BY  
DMS  
SIGNATURE  
P.E. NUMBER  
DATE

NEQUASSET BRIDGE  
NEQUASSET OUTLET  
SAGadahoc COUNTY  
WOOLWICH  
SUPERSTRUCTURE DETAILS

SHEET NUMBER  
11  
OF 15

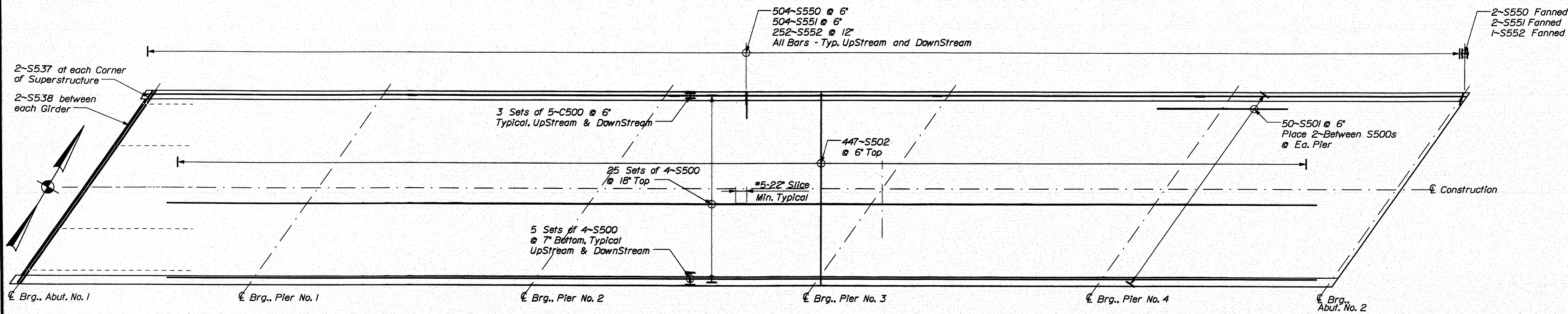


Date:11/7/2007

Username: david.sullivan

Division: BRIDGE

Filename: ...\\012\_Superstructure\_Rebar.dgn



SUPERSTRUCTURE REINFORCING STEEL PLAN

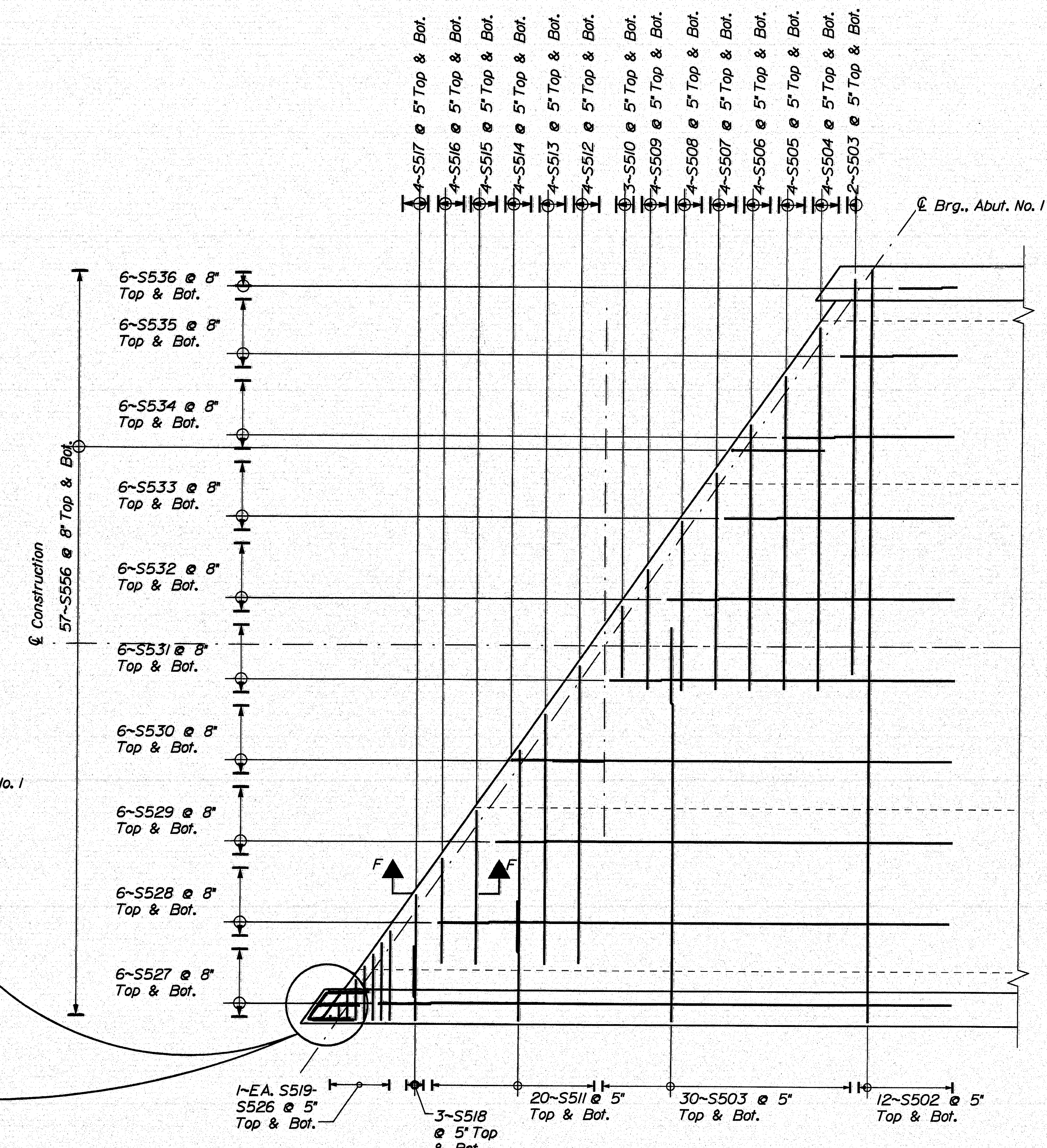
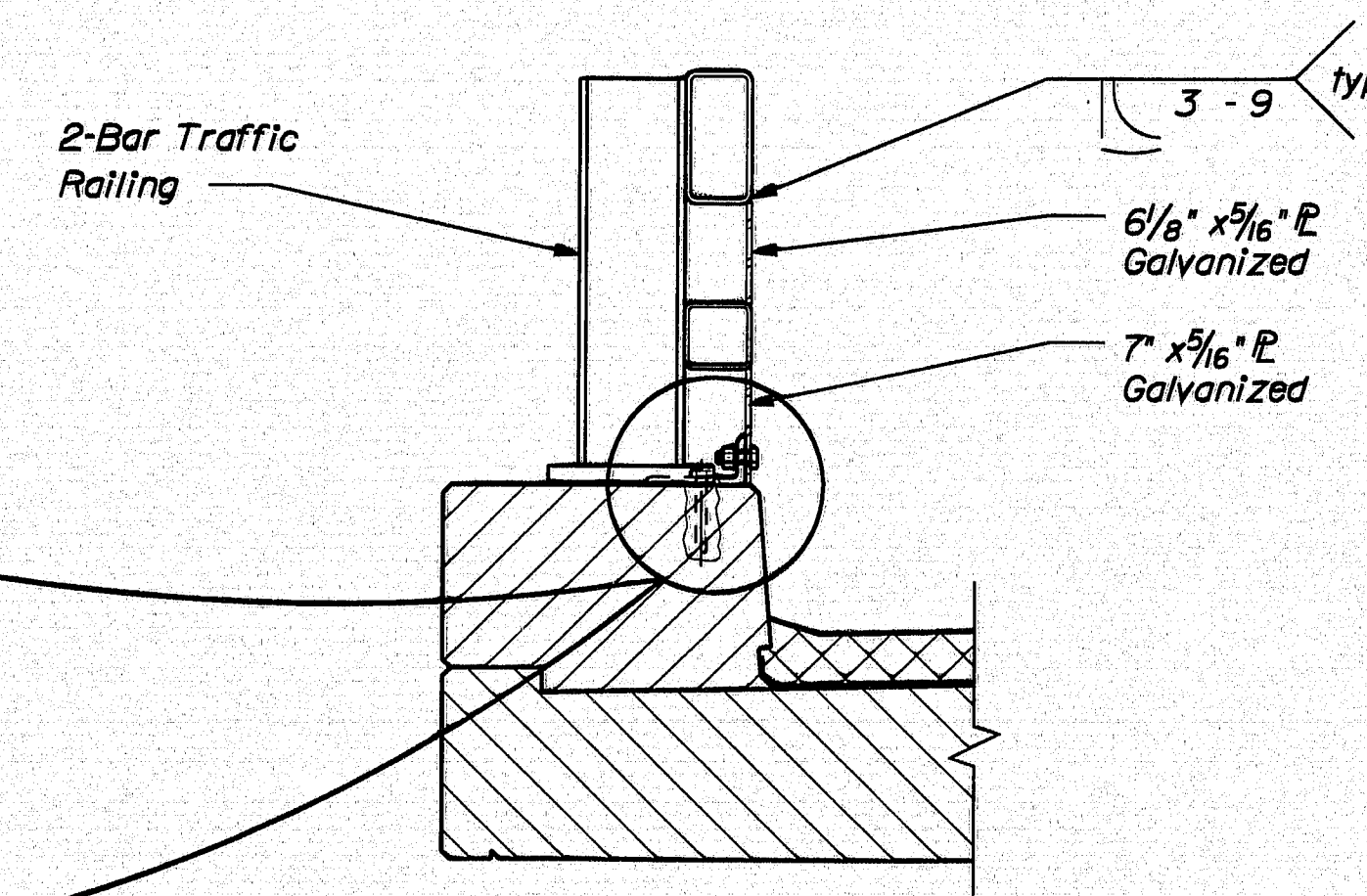
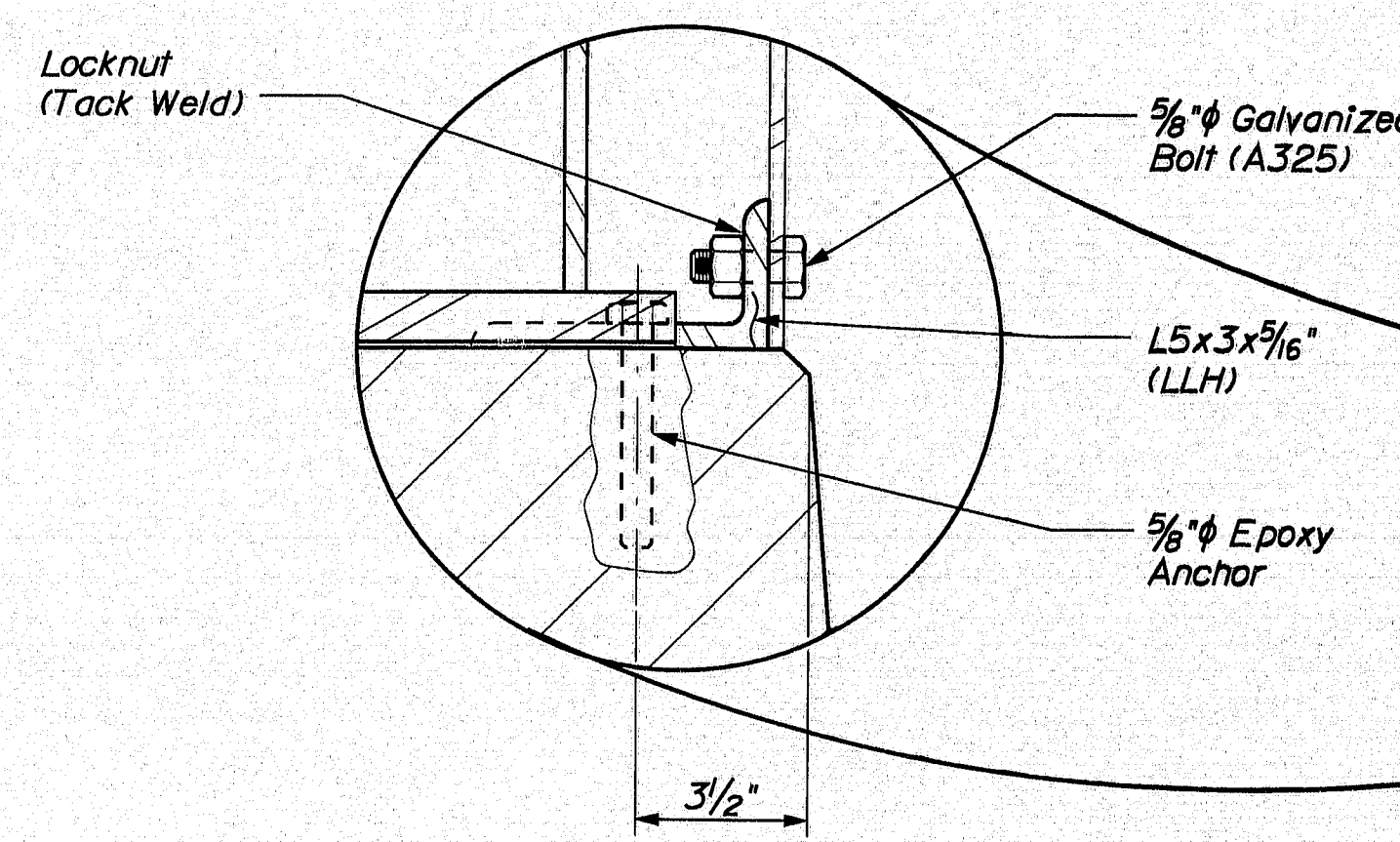
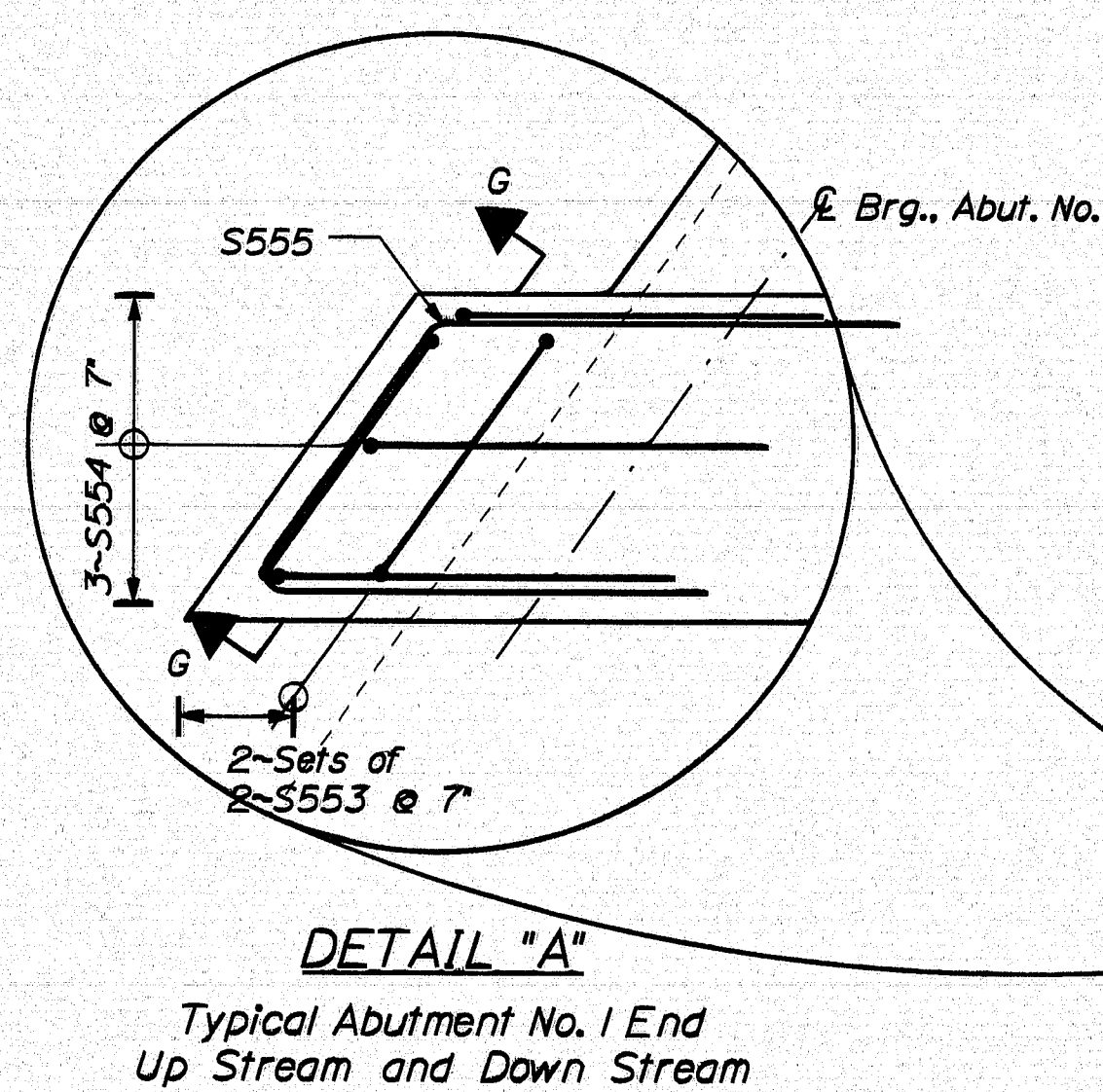
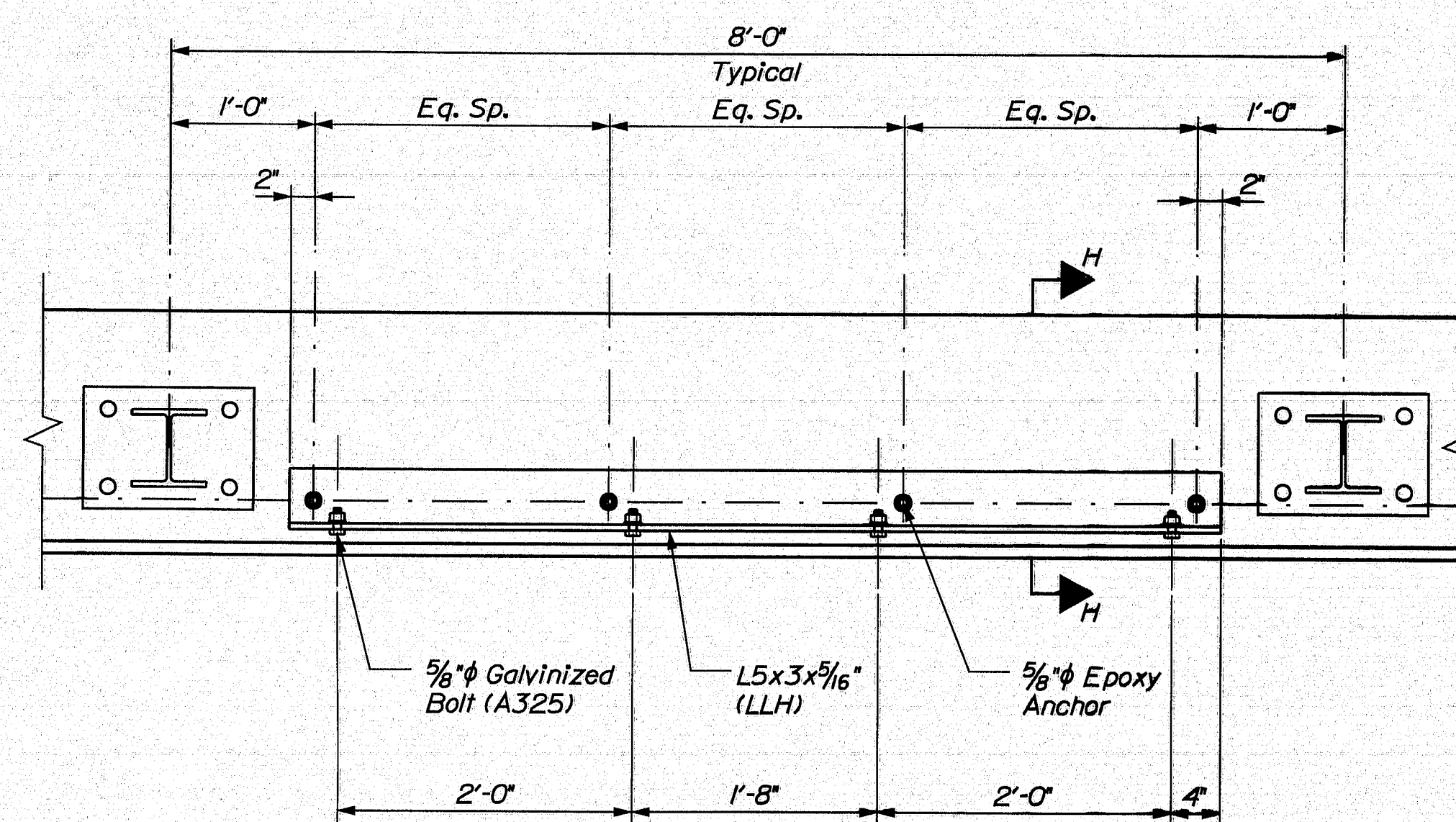
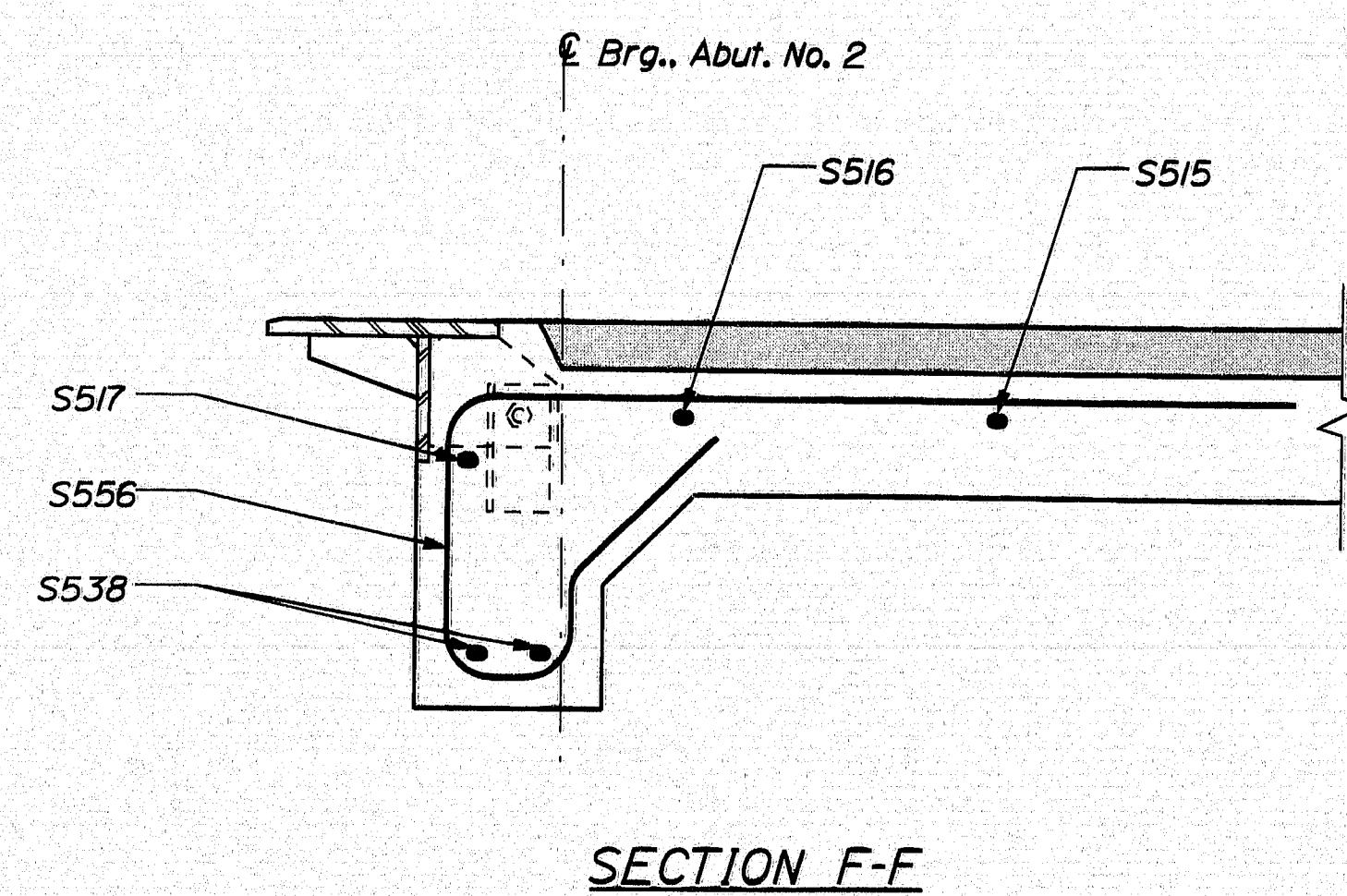
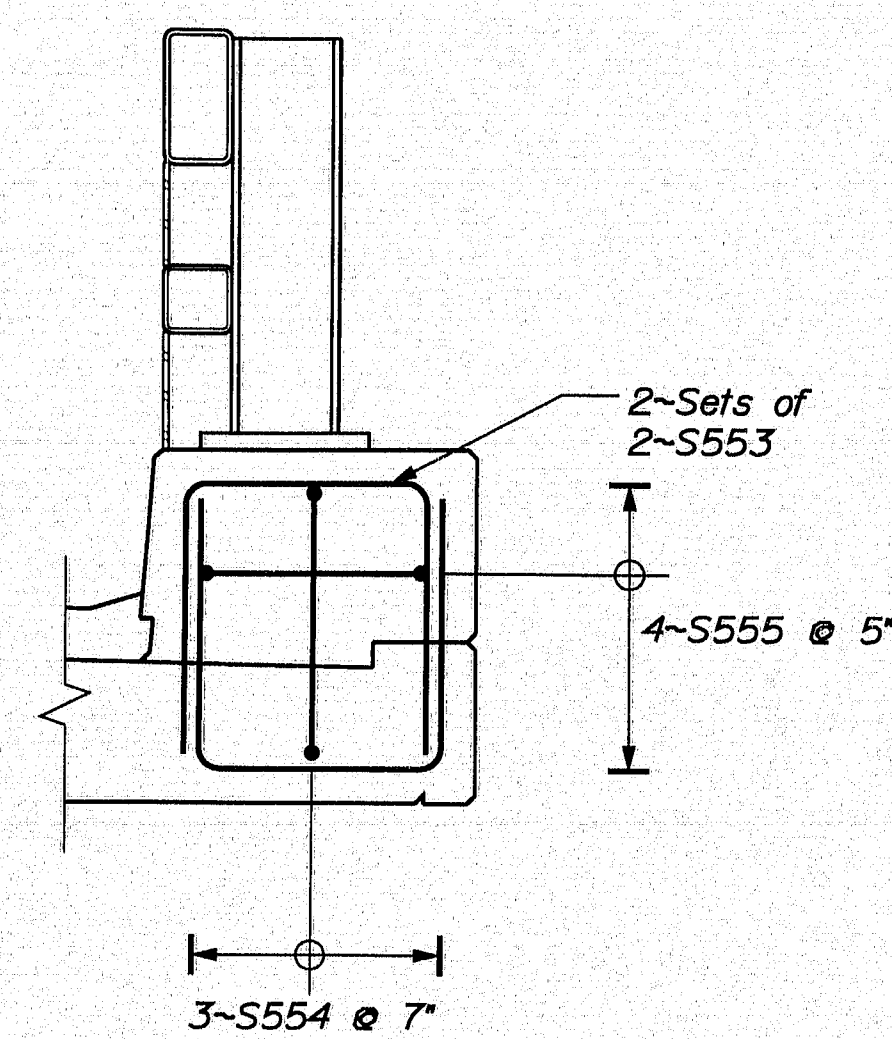
| STRAIGHT BARS  |      |         |                       |
|----------------|------|---------|-----------------------|
| MARK           | QTY. | LENGTH  | LOCATION              |
| SUPERSTRUCTURE |      |         |                       |
| S500           | 140  | 56'-6"  | Super & Curb Long.    |
| S501           | 200  | 25'-0"  | Superstructure Long.  |
| S502           | 471  | 37'-0"  | Superstructure Trans. |
| S503           | 64   | 19'-5"  | End Piece Trans.      |
| S504           | 8    | 17'-10" | End Piece Trans.      |
| S505           | 8    | 15'-6"  | End Piece Trans.      |
| S506           | 8    | 13'-1"  | End Piece Trans.      |
| S507           | 8    | 10'-8"  | End Piece Trans.      |
| S508           | 8    | 8'-4"   | End Piece Trans.      |
| S509           | 8    | 5'-11"  | End Piece Trans.      |
| S510           | 6    | 3'-6"   | End Piece Trans.      |
| S511           | 40   | 5'-11"  | End Piece Trans.      |
| S512           | 8    | 14'-8"  | End Piece Trans.      |
| S513           | 8    | 12'-4"  | End Piece Trans.      |
| S514           | 8    | 9'-11"  | End Piece Trans.      |
| S515           | 8    | 7'-7"   | End Piece Trans.      |
| S516           | 8    | 5'-2"   | End Piece Trans.      |
| S517           | 8    | 5'-0"   | End Piece Trans.      |
| S518           | 6    | 3'-8"   | End Piece Trans.      |
| S519           | 2    | 4'-6"   | End Piece Trans.      |
| S520           | 2    | 3'-10"  | End Piece Trans.      |
| S521           | 2    | 3'-3"   | End Piece Trans.      |
| S522           | 2    | 2'-8"   | End Piece Trans.      |
| S523           | 2    | 2'-0"   | End Piece Trans.      |
| S524           | 2    | 1'-6"   | End Piece Trans.      |
| S525           | 2    | 0'-8"   | End Piece Trans.      |
| S526           | 2    | 0'-3"   | End Piece Trans.      |
| S527           | 12   | 28'-0"  | End Piece Long.       |
| S528           | 12   | 25'-0"  | End Piece Long.       |
| S529           | 12   | 22'-4"  | End Piece Long.       |
| S530           | 12   | 19'-6"  | End Piece Long.       |
| S531           | 12   | 16'-10" | End Piece Long.       |
| S532           | 12   | 14'-0"  | End Piece Long.       |
| S533           | 12   | 11'-2"  | End Piece Long.       |
| S534           | 12   | 8'-4"   | End Piece Long.       |
| S535           | 12   | 5'-8"   | End Piece Long.       |
| S536           | 12   | 2'-10"  | End Piece Long.       |
| S537           | 8    | 2'-9"   | End Corners           |
| S538           | 12   | 9'-5"   | End Corners           |

| BENT BARS      |       |        |      |        |       |       |       |       |   |       |        |   |   |
|----------------|-------|--------|------|--------|-------|-------|-------|-------|---|-------|--------|---|---|
| MARK           | QTY.  | LENGTH | TYPE | A      | B     | C     | D     | E     | F | G     | H      | O | R |
| SUPERSTRUCTURE |       |        |      |        |       |       |       |       |   |       |        |   |   |
| S550           | 1,008 | 5'-2"  | SC   | 0'-10" | 1'-4" | 1'-3" | 1'-0" |       | * | 0'-9" | *      |   | * |
| S551           | 1,008 | 2'-11" | C    | 0'-8"  | 2'-3" |       |       |       | * |       | *      |   | * |
| S552           | 252   | 5'-7"  | C    | 0'-7"  | 5'-0" |       |       |       | * |       | *      |   | * |
| S553           | 4     | 4'-2"  | S    | 2'-4"  | 1'-4" | 1'-6" | 1'-4" |       |   |       |        |   |   |
| S554           | 6     | 5'-4"  | S    | 4'-6"  | 2'-0" | 1'-4" | 2'-0" |       |   |       |        |   |   |
| S555           | 8     | 6'-4"  | ZA   |        | 1'-8" | 2'-4" |       |       |   |       | 0'-11" |   |   |
| S556           | 114   | 8'-3"  | ZB   |        | 1'-6" | 0'-8" | 0'-7" | 1'-0" |   |       |        |   |   |

|                  |           |                                  |                  |
|------------------|-----------|----------------------------------|------------------|
| STATE OF MAINE   |           | DEPARTMENT OF TRANSPORTATION     |                  |
| BH-1262(800)X    |           | BRIDGE NO. 5695                  |                  |
| PIN 12628.00     |           | BRIDGE PLANS                     |                  |
| DATE             | SIGNATURE | P.E. NUMBER                      | DATE             |
| DESIGN-DETAILED  | RC        | DESIGN-DETAILED                  | DESIGN-DETAILED  |
| CHECKED-REVIEWED | RC        | CHECKED-REVIEWED                 | CHECKED-REVIEWED |
| DESIGN-DETAILED  | RC        | DESIGN-DETAILED                  | DESIGN-DETAILED  |
| REVISIONS 1      |           | REVISIONS 2                      |                  |
| REVISIONS 3      |           | REVISIONS 4                      |                  |
| FIELD CHANGES    |           | FIELD CHANGES                    |                  |
| NEQUASSET BRIDGE |           | SHEET NUMBER                     |                  |
| NEQUASSET OUTLET |           | 12                               |                  |
| WOOLWICH         |           | OF 15                            |                  |
| SAGadahoc COUNTY |           | SUPERSTRUCTURE REINFORCING STEEL |                  |

145-337





|                                     |                                    |  |  |  |  |  |  |  |  |  |                  |             |     |             |   |              |
|-------------------------------------|------------------------------------|--|--|--|--|--|--|--|--|--|------------------|-------------|-----|-------------|---|--------------|
| SHEET NUMBER<br><br>13<br><br>OF 15 | NEQUASSET BRIDGE                   |  |  |  |  |  |  |  |  |  | PROJ. MANAGER    | BEN FOSTER  | BY  | DATE        | STATE OF MAINE<br><br>DEPARTMENT OF TRANSPORTATION<br><br>BH-1262(900)X |              |
|                                     | NEQUASSET OUTLET                   |  |  |  |  |  |  |  |  |  | CHECKED-DRAWN    | RRC         | DMS | SIGNATURE   |   |              |
|                                     | WOOLWICH                           |  |  |  |  |  |  |  |  |  | DESIGNED-BY      | DESIGNED-BY |     | P.E. NUMBER |   |              |
|                                     | SAGADAHOC COUNTY                   |  |  |  |  |  |  |  |  |  | DESIGNED-CHECKED |             |     | DATE        |   |              |
|                                     | SUPERSTRUCTURE REINFORCING DETAILS |  |  |  |  |  |  |  |  |  | REVISIONS 1      |             |     |             |   |              |
|                                     |                                    |  |  |  |  |  |  |  |  |  | REVISIONS 2      |             |     |             |   |              |
|                                     |                                    |  |  |  |  |  |  |  |  |  | REVISIONS 3      |             |     |             |   |              |
|                                     |                                    |  |  |  |  |  |  |  |  |  | REVISIONS 4      |             |     |             |   |              |
|                                     |                                    |  |  |  |  |  |  |  |  |  | FIELD CHANGES    |             |     |             |   |              |
|                                     |                                    |  |  |  |  |  |  |  |  |  |                  |             |     |             |   |              |
| BRIDGE NO. 5695                     |                                    |  |  |  |  |  |  |  |  |  |                  |             |     | PIN         | 12623.00  | BRIDGE PLANS |

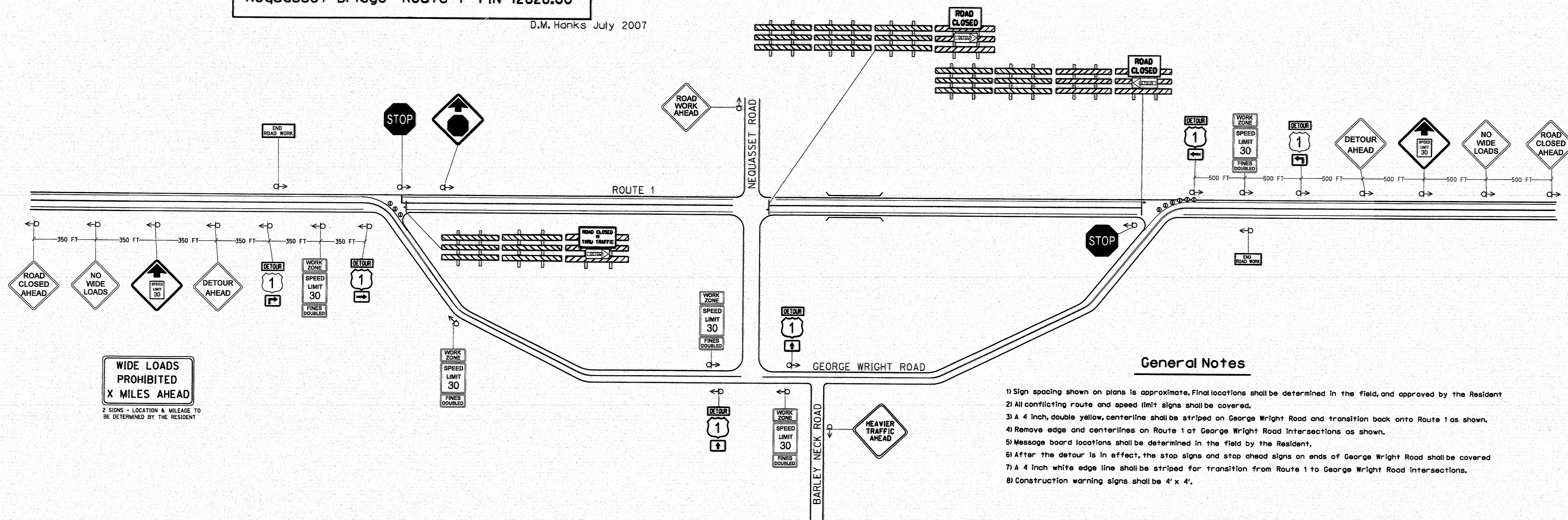


| STRAIGHT BARS |      |         |                         |      |      |        |          | BENT BARS |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|---------------|------|---------|-------------------------|------|------|--------|----------|-----------|------|--------|------|-------|--------|-------|--------|---|---|---|---|---|---|--------------|--------------|
| MARK          | QTY. | LENGTH  | LOCATION                | MARK | QTY. | LENGTH | LOCATION | MARK      | QTY. | LENGTH | TYPE | A     | B      | C     | D      | E | F | G | H | O | R | LOCATION     |              |
| ABUTMENT No.1 |      |         |                         |      |      |        |          | A650      | 41   | 6'-8"  | S    |       | 3'-0"  |       | 3'-0"  | * | * | * | * | * | * | *            | Longitudinal |
| 600           | 108  | 4'-10"  | Abutment Vertical Dowel | *    | *    | *      | *        | A651      | 4    | 7'-10" | S    |       | 3'-4"  | 0'-8" | 3'-4"  | * | * | * | * | * | * | *            | Longitudinal |
| 601           | 4    | 52'-4"  | Transverse Abutment     | *    | *    | *      | *        | A652      | 1    | 7'-2"  | S    |       | 3'-0"  | 1'-2" | 3'-0"  | * | * | * | * | * | * | *            | Longitudinal |
| 602           | 4    | 4'-3"   | Transverse Abutment     |      |      |        |          | A653      | 7    | 8'-10" | S    |       | 3'-10" | 1'-2" | 3'-10" |   |   |   |   |   |   |              | Longitudinal |
| 603           | 5    | 2'-9"   | Transverse Abutment     |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
| 604           | 1    | 3'-6"   | Transverse Abutment     |      |      |        |          | B650      | 48   | 4'-10" | S    |       | 1'-2"  |       | 2'-8"  |   |   |   |   |   |   |              | Longitudinal |
| 605           | 5    | 6'-6"   | Transverse Abutment     |      |      |        |          | B651      | 48   | 3'-10" | L    | 2'-6" | 1'-4"  | 1'-0" |        |   |   |   |   |   |   |              | Longitudinal |
| 606           | 1    | 5'-11"  | Transverse Abutment     |      |      |        |          | B652      | 7    | 6'-4"  | S    |       | 1'-10" |       | 3'-6"  |   |   |   |   |   |   |              | Longitudinal |
| ABUTMENT No.2 |      |         |                         |      |      |        |          | B653      | 7    | 4'-7"  | L    | 3'-3" | 1'-4"  | 1'-0" |        |   |   |   |   |   |   | Longitudinal |              |
| 600           | 2    | 28'-6"  | Transverse Abutment     |      |      |        |          | B654      | 6    | 6'-0"  | S    |       | 1'-8"  |       | 3'-4"  |   |   |   |   |   |   |              | Longitudinal |
| 601           | 1    | 6'-9"   | Transverse Abutment     |      |      |        |          | B655      | 6    | 4'-6"  | L    | 3'-2" | 1'-4"  | 1'-0" |        |   |   |   |   |   |   |              | Longitudinal |
| 602           | 1    | 5'-7"   | Transverse Abutment     |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
| 603           | 1    | 5'-11"  | Transverse Abutment     |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
| 604           | 1    | 6'-0"   | Transverse Abutment     |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
| APPROACH SLAB |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
| 400           | 24   | 21'-0"  | Transverse              |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
| 600           | 80   | 15'-11" | Longitudinal            |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
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|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
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|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
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|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
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|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   |              |              |
|               |      |         |                         |      |      |        |          |           |      |        |      |       |        |       |        |   |   |   |   |   |   | </           |              |



# Traffic Control Plan - Woolwich Nequasset Bridge- Route 1- PIN 12628.00

D.M. Hanks July 2007



|                  |              |                              |     |
|------------------|--------------|------------------------------|-----|
| STATE OF MAINE   |              | DEPARTMENT OF TRANSPORTATION |     |
| BH-1262(800)X    |              | P.E. NUMBER                  |     |
| BRIDGE NO. 5695  | PIN 12628.00 | BRIDGE PLANS                 |     |
| PROJECT MANAGER  | BEN FOSTER   | DATE                         |     |
| DESIGN DETAIL    | RC           | BY                           | DMS |
| CHECKED-REVIEWED |              | SIGNATURE                    |     |
| DESIGN-DETAILED  |              | P.E. NUMBER                  |     |
| REVISIONS 1      |              | DATE                         |     |
| REVISIONS 2      |              |                              |     |
| REVISIONS 3      |              |                              |     |
| REVISIONS 4      |              |                              |     |
| FIELD CHANGES    |              |                              |     |
| NEQUASSET BRIDGE |              | SAGadahoc COUNTY             |     |
| NEQUASSET OUTLET |              | WOOLWICH                     |     |
| DETOUR PLAN      |              | SHEET NUMBER                 |     |
|                  |              | 15                           |     |
|                  |              | OF 15                        |     |

145-340



STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION



**WOOLWICH**

Sagadahoc

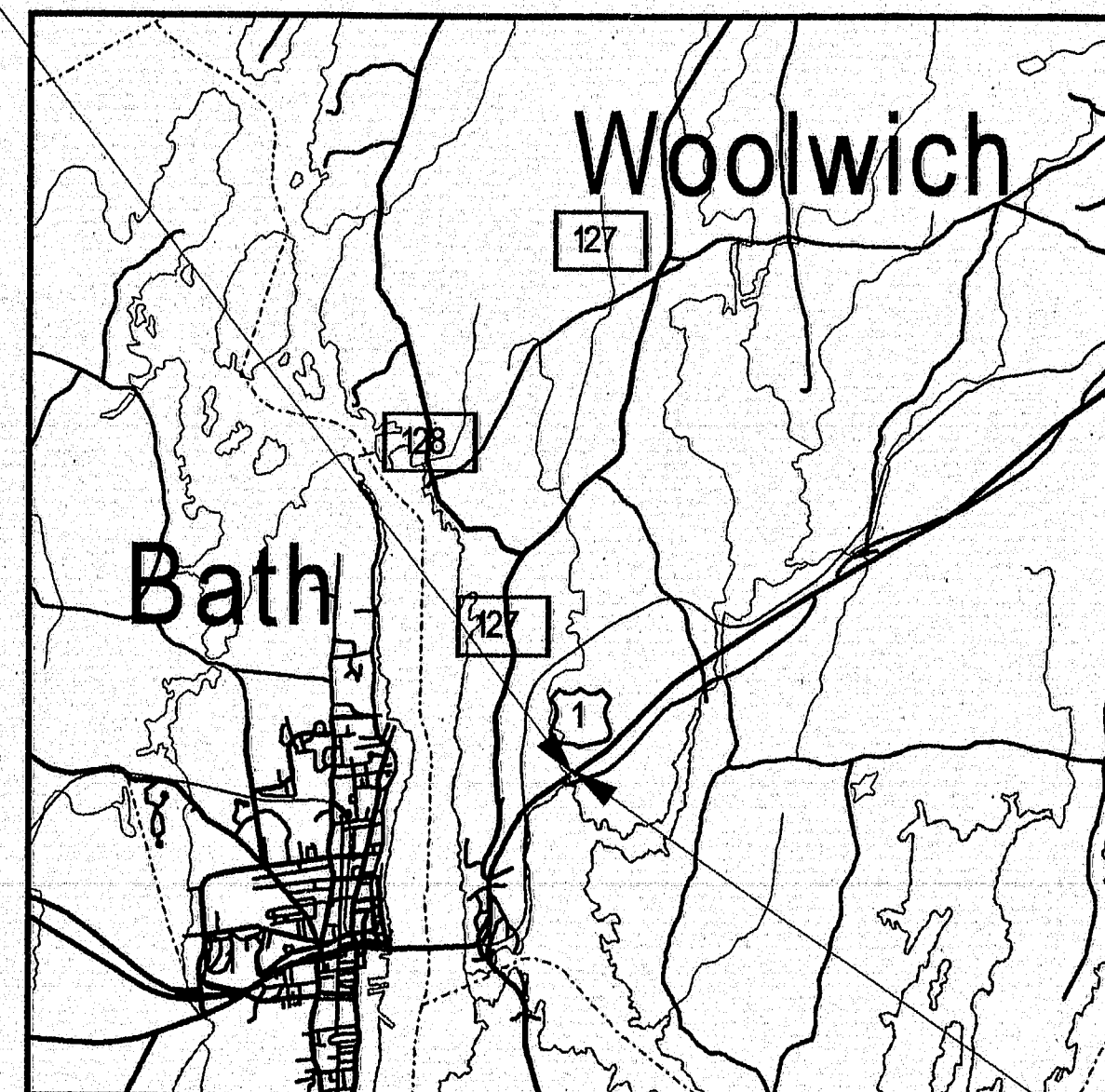
U.S. Route 1

**PIN 14950.00**

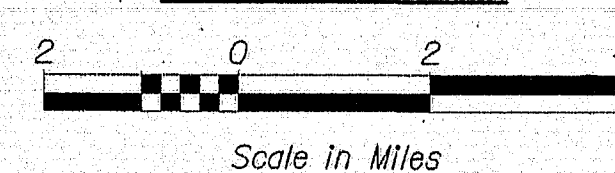
PROJECT LENGTH : 112 ft.

CULVERT REPLACEMENT

BEGIN PROJECT  
PIN 14950.00



LOCATION MAP



END PROJECT  
PIN 14950.00

BEGIN PROJECT  
LIMITS STA.10+39.5

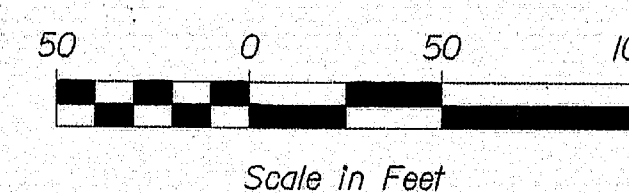
TO BATH

U.S. ROUTE 1

TO WISCASSET

END PROJECT  
LIMITS STA.11+51.5

LAYOUT SCALE



GEO. WRIGHT RD.

PLAN LEGEND

|                          |     |                                 |                          |
|--------------------------|-----|---------------------------------|--------------------------|
| EXISTING CENTERLINE      | --- | LEDGE                           | ~~~~~                    |
| PROPOSED CENTERLINE      | --- | EXISTING DRAINAGE               | ---                      |
| TRAVELWAY - EXISTING     | --- | PROPOSED DRAINAGE               | ---                      |
| TRAVELWAY - PROPOSED     | --- | EXISTING GUARDRAIL              | ---                      |
| PROPERTY LINES           | --- | PROPOSED GUARDRAIL              | ---                      |
| R/W LINES EXISTING       | --- | CATCH BASINS                    | □ EXISTING    ● PROPOSED |
| R/W LINES PROPOSED       | --- | MANHOLES                        | ○ EXISTING    ● PROPOSED |
| CULVERT EXISTING         | --- | UTILITY POLES                   | ○ EXISTING    ● PROPOSED |
| CULVERT PROPOSED         | --- | FIRE HYDRANTS                   | ○ EXISTING    ● PROPOSED |
| EXISTING WATERLINE       | --- | TREES                           | ⊗ HARDWOOD    ⊗ SOFTWOOD |
| EXISTING SAN-SEWER       | --- | ● SOLID STEM AUGER/CASED BORING |                          |
| UNDERDRAIN / STORM DRAIN | --- | ● POWER AUGER PROBE             |                          |
| OUTLINE BODIES OF WATER  | --- | ● PAVEMENT CORE                 |                          |
| STREAM / CREEK           | --- | CURB - EXISTING                 | ---                      |
| WOODS                    | --- | CURB - PROPOSED                 | ---                      |
| CLEARING LIMIT LINE      | --- |                                 |                          |
| RAILROAD                 | --- |                                 |                          |
| FENCE (TYPE )            | --- |                                 |                          |

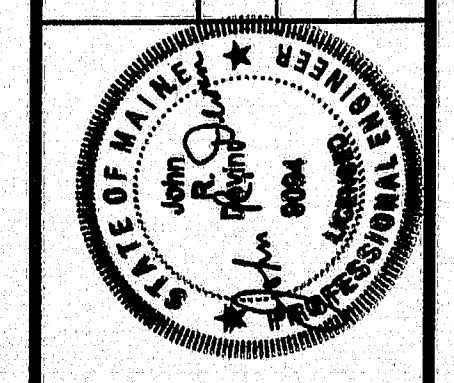
**TRAFFIC DATA**

|                                |       |
|--------------------------------|-------|
| Current (2005) AADT            | 20550 |
| Future (2025) AADT             | 28770 |
| DHV - % of AADT                | 10%   |
| Design Hour Volume             | 2877  |
| % Heavy Trucks (AADT)          | 5%    |
| % Heavy Trucks (DHV)           | 4%    |
| Directional Distribution (DHV) | 58%   |
| 18 kip Equivalent P 2.0        | 583   |
| 18 kip Equivalent P 2.5        | 556   |

**INDEX OF SHEETS**

| Description                  | Sheet No. |
|------------------------------|-----------|
| Title Sheet                  | 1         |
| Typical Sections             | 2         |
| Estimated Quantities & Notes | 3         |
| Plan                         | 4         |
| Cross - Sections             | 5-7       |

|  |          |          |
|--|----------|----------|
| STATE OF MAINE<br>DEPARTMENT OF TRANSPORTATION | APPROVED | DATE     |
| COMMISSIONER                                   | 11/13/07 | 11/13/07 |
| CHIEF ENGINEER                                 |          |          |



|           |      |
|-----------|------|
| Signature | DATE |
| 11/13/07  |      |

|                         |                 |
|-------------------------|-----------------|
| PROJECT INFORMATION     | PROGRAM         |
| PROJECT MANAGER         | JOEL KITTEDGE   |
| DESIGNER                | RANDALL BARROWS |
| CONSULTANT              |                 |
| PROJECT RESIDENT        |                 |
| CONTRACTOR              |                 |
| PROJECT COMPLETION DATE |                 |

PIN 14950.00

WOOLWICH  
U.S. Route 1

TITLE SHEET

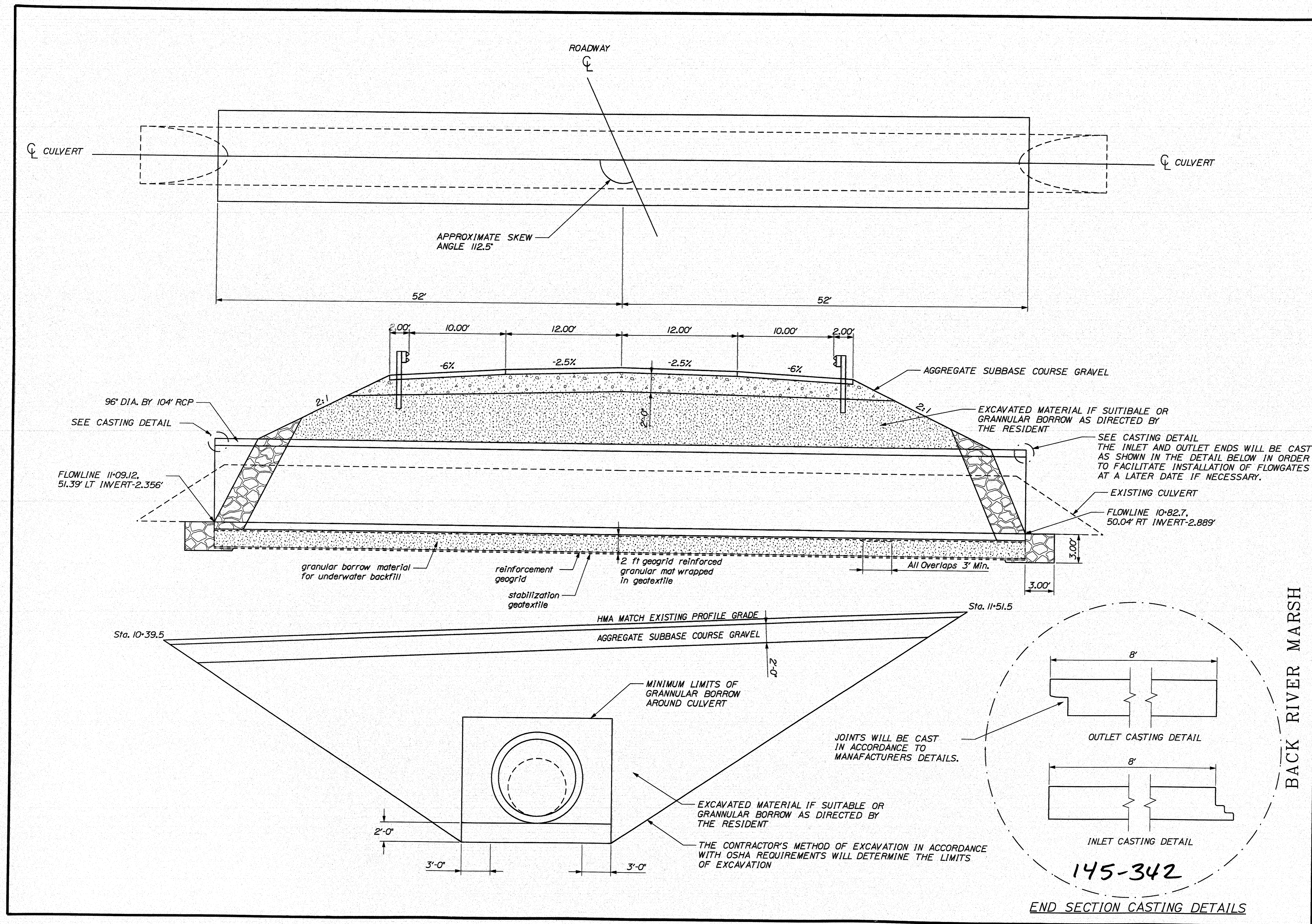
SHEET NUMBER

1

OF 7

145 - 341





|           |                   |  |  |                   |             |      |
|-----------|-------------------|--|--|-------------------|-------------|------|
| 2<br>OF 7 | SHEET NUMBER      | WOOLWICH<br>U.S. ROUTE 1<br><br>TYPICAL SECTIONS | PROJ. MANAGER  | JOSEPH R. BRIDGES | BY          | DATE |
|           |                   |  | DESIGN-DETAILED  | R. BRIDGES        |             |      |
|           | CHECKED-REVIEWED  |  |  |                   | SIGNATURE   |      |
|           | DESIGNED-DETAILED |  |  |                   |             |      |
|           | REVISIONS 1       |  |  |                   | P.E. NUMBER |      |
|           | REVISIONS 2       |  |  |                   |             |      |
|           | REVISIONS 3       |  |  |                   | DATE        |      |
|           | REVISIONS 4       |  |  |                   |             |      |
|           | FIELD CHANGES     |  |  |                   |             |      |
|           |                   |  | STATE OF MAINE<br>DEPARTMENT OF TRANSPORTATION<br><br>14950.00<br><br>PIN<br>14950.00<br>HIGHWAY PLANS |                   |             |      |



| ESTIMATED QUANTITIES |  |          |      |
|----------------------|--|----------|------|
| ITEM NO.             | DESCRIPTION                              | QUANTITY | UNIT |
| 201.11               | CLEARING                                 | .15      | AC   |
| 203.20               | COMMON EXCAVATION                        | 500      | CY   |
| 203.25               | GRANULAR BORROW                          | 2750     | CY   |
| 206.061              | STR EARTH BELOW GRADE STR                | 50       | CY   |
| 304.10               | AGGR SUBB COURSE - GRAVEL                | 430      | CY   |
| 403.210              | HOT MIX ASPHALT 9.5 MM HMA               | 50       | T    |
| 403.213              | HOT MIX ASPHALT 12.5 MM HMA BASE         | 110      | T    |
| 409.15               | BITUMINOUS TACK COAT APPLIED             | 30       | G    |
| 511.07               | COFFERDAM:UPSTREAM                       | 1        | LS   |
| 511.07               | COFFERDAM:DOWNSTREAM                     | 1        | LS   |
| 526.30               | TEMPORARY CONCRETE BARRIER - TYPE I      | 100      | LF   |
| 603.531              | 96" REINFORCED CONCRETE PIPE - CLASS IV  | 1        | LS   |
| 606.36               | GR REMOVED AND RESET                     | 325      | LF   |
| 610.08               | PLAIN RIPRAP                             | 100      | CY   |
| 618.141              | SEEDING METHOD NUMBER 3 - PLAN QUANTITY  | 4        | UN   |
| 619.12               | MULCH                                    | 4        | UN   |
| 620.54               | STABILIZATION GEOTEXTILE                 | 450      | SY   |
| 620.58               | EROSION CONTROL GEOTEXTILE               | 140      | SY   |
| 620.65               | REINFORCEMENT GEOGRID                    | 400      | SY   |
| 627.711              | WH OR YELL PAINT PYMT MARKING LINE       | 240      | LF   |
| 627.76               | TEMPORARY PYMT. MARK LINE, W OR YELLOW   | 1        | LS   |
| 652.312              | TYPE III BARRICADE                       | 10       | EA   |
| 652.33               | DRUM                                     | 10       | EA   |
| 652.34               | CONE                                     | 20       | EA   |
| 652.35               | CONSTRUCTION SIGNS                       | 161      | SF   |
| 652.36               | MAINT OF TRAFFIC CONTR DEVICES           | 21       | CD   |
| 652.38               | FLAGGER                                  | 200      | HR   |
| 656.75               | TEMP. SOIL EROS. AND WATER POLL. CONTROL | 1        | LS   |
| 659.10               | MOBILIZATION                             | 1        | LS   |
|                      |  |          |      |
|                      |  |          |      |
|                      |  |          |      |

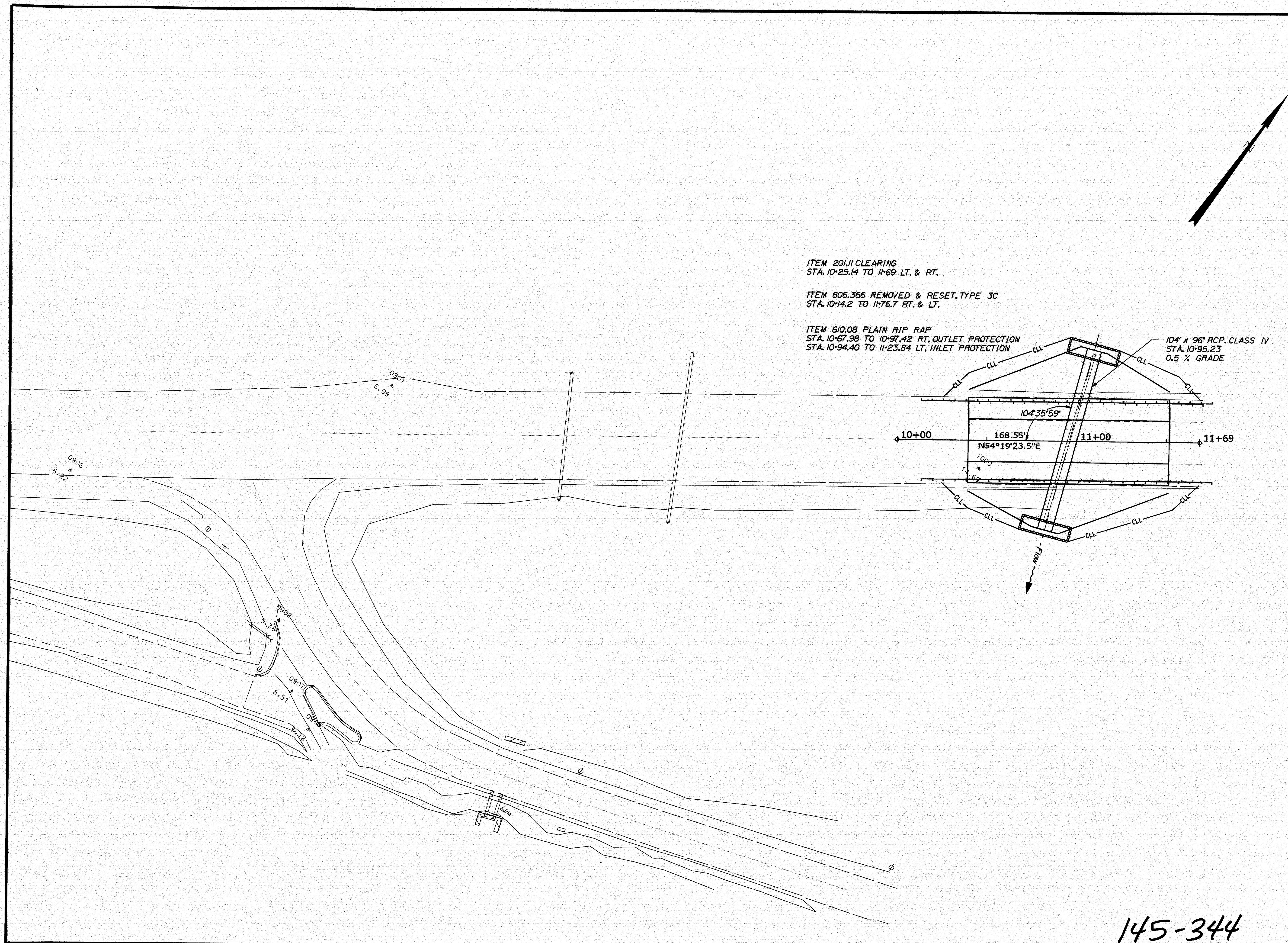
GENERAL NOTES

1. NO UTILITY INVOLVEMENT IS ANTICIPATED.
2. THE CLEARING SHOWN ON THE PLANS ARE FOR ESTIMATING PURPOSES ONLY. THE ACTUAL LINES FOR PAYMENT SHALL BE ESTABLISHED IN THE FIELD BY THE RESIDENT.
3. IF FOUNDATION MATERIAL IS REQUIRED UNDER CULVERTS, IT SHALL MEET THE REQUIREMENTS FOR GRANULAR BORROW - UNDERWATER BACKFILL AND WILL BE PAID FOR AS GRANULAR BORROW.
4. GRANULAR BORROW USED TO BACKFILL MUCK EXCAVATION OR IN LOW WET AREAS TO 1' ABOVE WATER LEVEL OR OLD GROUND SHALL MEET REQUIREMENTS FOR GRANULAR BORROW UNDERWATER BACKFILL.
5. ANY DAMAGE TO THE SLOPES CAUSED BY THE CONTRACTOR'S EQUIPMENT, PERSONNEL OR OPERATION SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT. ALL WORK, EQUIPMENT, AND MATERIALS REQUIRED TO MAKE REPAIRS SHALL BE AT THE CONTRACTOR'S EXPENSE.
6. TEMPORARY EROSION CONTROL MEASURES SHALL BE MAINTAINED AS SPECIFIED IN THE SOIL EROSION AND WATER POLLUTION CONTROL PLAN. AND THE MEDOT BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL JAN 2000. PAYMENT WILL BE MADE UNDER 656.75, TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL, LUMP SUM.
7. EXCAVATIONS ACCOMPLISHED AS PART OF THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH SUBPART P OF 29 CFR PART 1926.650-.652 (CONSTRUCTION STANDARD FOR EXCAVATIONS).
8. DURING CONSTRUCTION, THE ROAD WILL BE CLOSED TO TRAFFIC FOR A TIME PERIOD SPECIFIED IN THE SPECIAL PROVISIONS.
9. A 2 FOOT THICK GRANULAR MAT WRAPPED IN GEOTEXTILE AND REINFORCED WITH A GEOGRID WILL BE PLACED UNDER THE PIPE. THE TOTAL AREA OF THE MAT SHALL BE SUFFICIENT TO EXTEND 3 FEET BEYOND THE FOOTPRINT OF THE PIPE AND THE TOTAL LENGTH OF THE GEOTEXTILE SHALL BE SUFFICIENT TO WRAP A 2 FOOT MAT THICKNESS WITH A 3 FOOT OVERLAP AT EACH SIDE AND END.
10. THE CONTRACTOR WILL MAINTAIN STREAM FLOW THROUGH THE TWO EXISTING CULVERTS TO THE SOUTH OF THE PROJECT.
11. MEDOT WILL FINAL STRIPE THE PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR TRANSFERRING THE EXISTING STIPING PATTERN TO THE SURFACE PAVEMENT.
12. ALL WASTE MATERIAL NOT USED ON THE PROJECT SHALL BE DISPOSED OF OFF THE PROJECT IN WASTE AREAS APPROVED BY THE RESIDENT.
13. ANY DREDGE MATERIAL WILL BE DISPOSED OF AS STATED IN SPECIAL PROVISION 203 DREDGE SPOILS.
14. ALL JOINTS BETWEEN EXISTING AND PROPOSED HOT BITUMINOUS PAVEMENT SHALL BE CUT ALONG A SMOOTH LINE PRODUCING A NEAT EVEN VERTICAL LINE. PAYMENT SHALL BE INCIDENTAL TO RELATED CONTRACT ITEMS.
15. THE EXPOSED INLET AND OUTLET PIPE SECTIONS WILL BE CAST WITHOUT OFFSET JOINTS ON THE OUTERMOST ENDS AS SHOWN IN THE PIPE DETAILS IN ORDER TO FACILITATE THE INSTALATION OF A FLOW GATE AT A LATER DATE IF NECESSARY.

|                          |  |                              |  |             |  |              |  |                                 |  |
|--------------------------|--|------------------------------|--|-------------|--|--------------|--|---------------------------------|--|
| STATE OF MAINE           |  | DEPARTMENT OF TRANSPORTATION |  | 14950.00    |  | PIN 14950.00 |  | HIGHWAY PLANS                   |  |
| WOOLWICH<br>U.S. Route 1 |  | SIGNATURE                    |  | P.E. NUMBER |  | DATE         |  | ESTIMATED QUANTITIES<br>& NOTES |  |
|                          |  |                              |  |             |  |              |  |                                 |  |
| SHEET NUMBER             |  | 3                            |  | OF 7        |  |              |  |                                 |  |

145-343





ITEM 20111 CLEARING  
STA. 10+25.14 TO 11+69 LT. & RT.

ITEM 606.366 REMOVED & RESET, TYPE 3C  
STA. 10+14.2 TO 11+76.7 RT. & LT.

ITEM 610.08 PLAIN RIP RAP  
STA. 10+67.98 TO 10+97.42 RT, OUTLET PROTECTION  
STA. 10+94.40 TO 11+23.84 LT, INLET PROTECTION

104" x 96" RCP, CLASS IV  
STA. 10+95.23  
0.5 % GRADE

145-344

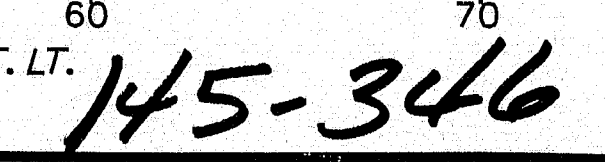
|                  |               |                              |      |
|------------------|---------------|------------------------------|------|
| STATE OF MAINE   |               | DEPARTMENT OF TRANSPORTATION |      |
| WOOLWICH         |               | U.S. Route 1                 |      |
| PLANS            |               | SHEET NUMBER                 |      |
| 4                |               | OF 7                         |      |
| 14950.00         |               | PIN                          |      |
| 14950.00         |               | HIGHWAY PLANS                |      |
| PROJ. MANAGER    | Joe Kittredge | BY                           | DATE |
| CHECKED-REVIEWED | R. Barrows    | SIGNATURE                    |      |
| DESIGN-DETAILS   |               | P.E. NUMBER                  |      |
| REVISIONS 1      |               | DATE                         |      |
| REVISIONS 2      |               |                              |      |
| REVISIONS 3      |               |                              |      |
| REVISIONS 4      |               |                              |      |
| FIELD CHANGES    |               |                              |      |







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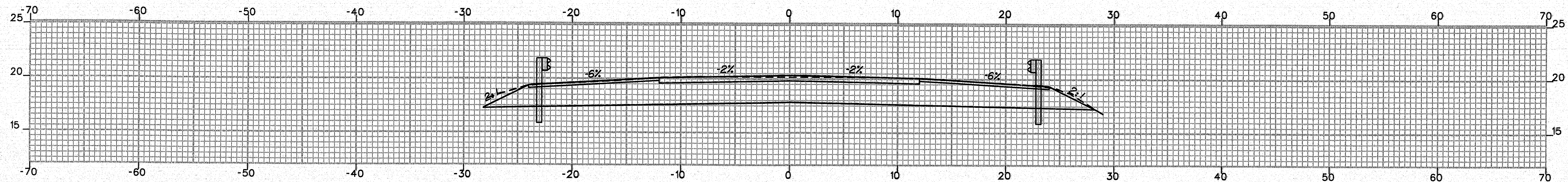


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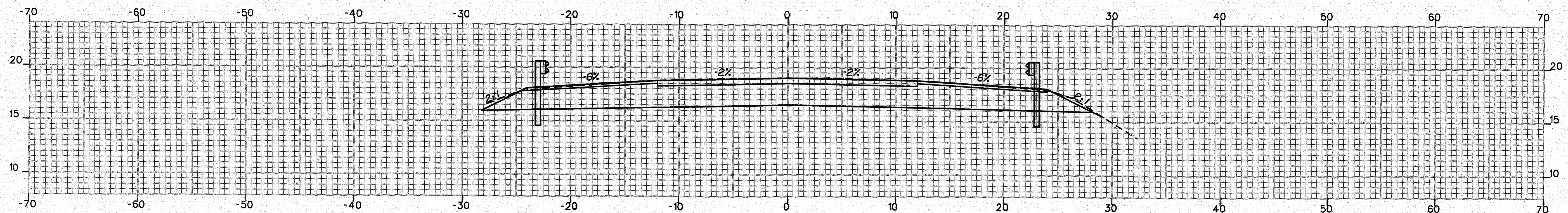
Username: coy.williams

Division: BRIDGE

Filename: ...\\msta\007\_XSMC10\_dr\_004.dgn



11+50.00  
End Construction Limits Station 11+51.5



11+25.00

|  |  |                                   |  |                 |  |                 |  |
|--|--|-----------------------------------|--|-----------------|--|-----------------|--|
| STATE OF MAINE<br>DEPARTMENT OF TRANSPORTATION |  | 14950.00                          |  | PIN<br>14950.00 |  | HIGHWAY PLANS   |  |
| WOOLWICH<br>U.S. Route 1                       |  | SIGNATURE                         |  | P.E. NUMBER     |  | DATE            |  |
| 11+25.00 CROSS SECTIONS 11+50.00               |  | PROJECT MANAGER<br>JOEL KITTREDGE |  | BY<br>E. BROWN  |  | DATE            |  |
| SHEET NUMBER                                   |  | DESIGN-REVIEWED                   |  | DESIGN-REVIEWED |  | DESIGN-REVIEWED |  |
| 7  |  | REVISIONS 1                       |  | REVISIONS 2     |  | REVISIONS 3     |  |
| OF 7   |  | REVISIONS 4                       |  | REVISIONS 5     |  | REVISIONS 6     |  |
| 145-347  |  | FIELD CHANGES                     |  | FIELD CHANGES   |  | FIELD CHANGES   |  |